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# Broadmoor Master Plan and Development Regulations

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City of Pasco

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March, 2023

Prepared by Oneza & Associates  
In partnership with White Bluffs Consulting

## Acknowledgements

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Appendix A: Environmental Impact Statement

Photo sources: City of Pasco, Oneza & Associates, Google Inc.

# 1 BACKGROUND AND EXISTING CONDITIONS

**You will find in this Section:**

- Introduction
- Existing conditions such as existing land use, zoning, topography, ownership, access, parks and open space, wildlife and habitats
- Assets and Constraints

## 1.1 INTRODUCTION

### 1.1.1 PURPOSE OF THE PLAN

The City of Pasco has been experiencing rapid growth over the last two decades. The population of the City has grown from 32,066 in 2000 to 59,781 in 2010. According to the Office of Financial Management estimate, the 2022 population of Pasco is 80,180. This is a 34% increase from its population in 2010. According to the Office of Financial Management estimates, the City's population is estimated to reach 121,828 in 2038. This will be a 52% increase from the City's 2022 population. With its growth, the City needs to provide adequate housing, jobs, infrastructure, and amenities for its future residents. The Broadmoor area is expected to hold a major share of the City's future growth in its approximately 1,240 acres of underutilized and vacant land. The purpose of the Broadmoor Master Plan is to provide a framework for the coordinated and long-term development in this area. It also provides guidelines for regulatory compliance for developments.

The Broadmoor area is intended to develop as a master-planned mixed-use pedestrian-friendly community with a variety of jobs and housing opportunities to meet the needs of the residents. The Broadmoor Area Master Plan (the Plan) will integrate the built and natural environments in a way that protects the environment yet provides amenities the community needs such as parks, shops, and schools.

*... accommodate higher density mixed-use, commercial, non-residential, and residential developments to accommodate the City's future growth.*

The purpose of the plan is to:

- Create a community with a mix of complementary developments
- Adjust current land use of the Broadmoor area to accommodate higher density mixed-use, commercial, non-residential, and residential developments to accommodate the City's future growth
- Provide adequate public facilities and infrastructure for the area to be developed
- Re-allocate open space locations based on recent changes in irrigation water management and an area-wide mitigation strategy for shrub-steppe habitat
- Provide a reliable transportation system that allows for a variety of mobility options for both motorized and non-motorized users
- Provide necessary preservation and mitigation for the natural environment and habitat areas

### 1.1.2 LOCATION AND CONTEXT

The area is located at the northwest side of Pasco, bounded by the Columbia River to the west, Broadmoor Boulevard to the east, and Interstate 182 (I-182) to the south. It is located more or less in the geographic center of the Tri-Cities region. Figure 1-1 shows the area and the vicinity.



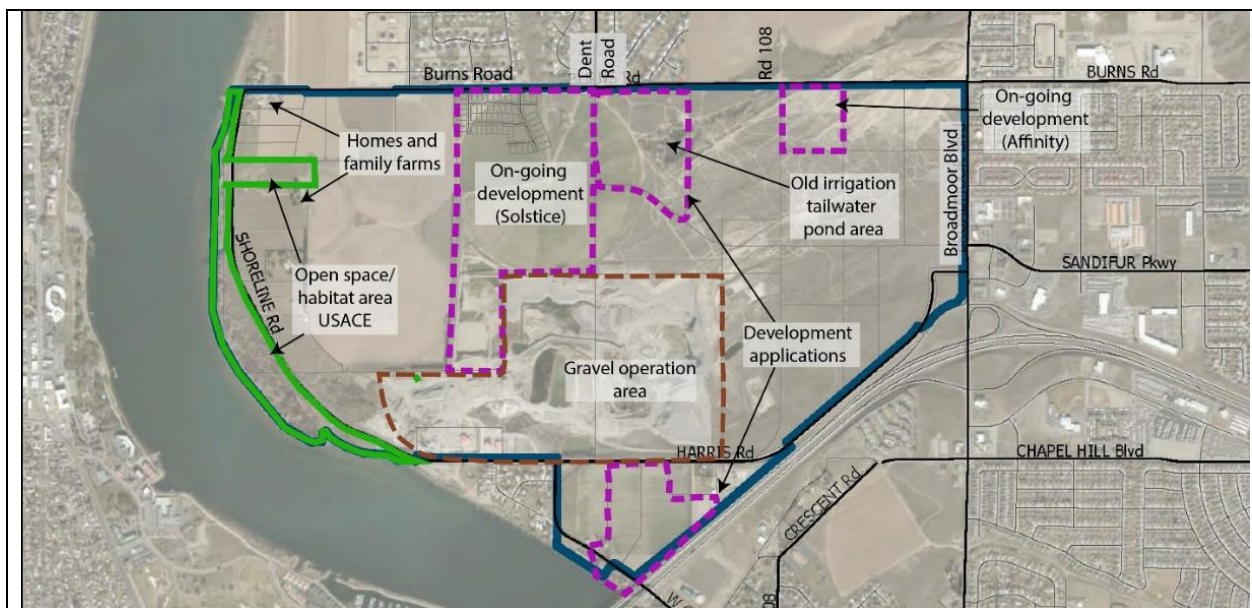
**FIGURE 1-1: BROADMOOR AREA AND VICINITY**

## 1.2 EXISTING CONDITIONS

This section describes existing conditions of the Broadmoor area in terms of land use, zoning, ownership, topography, infrastructure, environmental conditions etc. Existing condition analysis in this section is updated as of December 12, 2022.

### 1.2.1 EXISTING LAND USES

The Broadmoor area is mostly undeveloped. A small portion of this area is used for agricultural activities by multiple owners. Approximately 250 acres are already in the process of various housing developments (Affinity & Hydro Broadmoor Apartments, Solstice, Dunes, and Columbia Shores).



**FIGURE 1-2: CURRENT LAND USES**

About 300 acres are devoted to mineral extraction (Central Pre-Mix). Approximately 58 acres of land is owned by the U.S. Army Corps of Engineers in the Columbia River shoreline and is designated Open Space. Approximately 800 acres are currently vacant, undeveloped, or used by small family farms. The area north of Burns Road outside the Broadmoor area is being developed with newer residential subdivisions. The City has been experiencing an ongoing development interest from the development community on the vacant areas.

Recent changes in irrigation water management leave the previous tailwater pond unused. The Central Pre-Mix operates under special permits that include approximately 300 acres of land for sand/gravel extraction, rock crushing, concrete processing, concrete batch plant operations, hot mix asphalt plant operations, and other related activities. The mining permit is expected to expire in 2025 without renewal.

The vacant lands abutting Broadmoor Boulevard to the west have been used by Outdoor Recreational Vehicle (ORV) enthusiasts for four-wheeling and dirt biking for decades. The natural environment in this portion of the Broadmoor area has been severely degraded by such activities. The area abutting the Columbia River, west of Shoreline Road, is identified as natural resources area owned by the U.S. Army Corps of Engineers. Figure 1-2 indicates current and ongoing land uses as of the end of the year 2022.

### **1.2.2 COMPREHENSIVE PLAN LAND USE AND ZONING**

The existing Comprehensive Plan land use for the Broadmoor area currently has a mix of medium to medium-high-density residential land uses throughout the area, with commercial designations along Harris Road and mixed-use and office designations along Broadmoor Boulevard, Sandifur Parkway, and Burns Road. Mixed land use is also designated on the west side of the Broadmoor area abutting the Shoreline Road. Open space areas are located along historical irrigation drainage areas, even though this drainage has recently been re-routed to the Columbia River. Recent developments and approved plans will result in significantly reduced densities and non-conformance with the current medium density land uses.

Figure 1-3 indicates land use within the site and vicinity. Table 1-1 indicates land use acreages in the area.



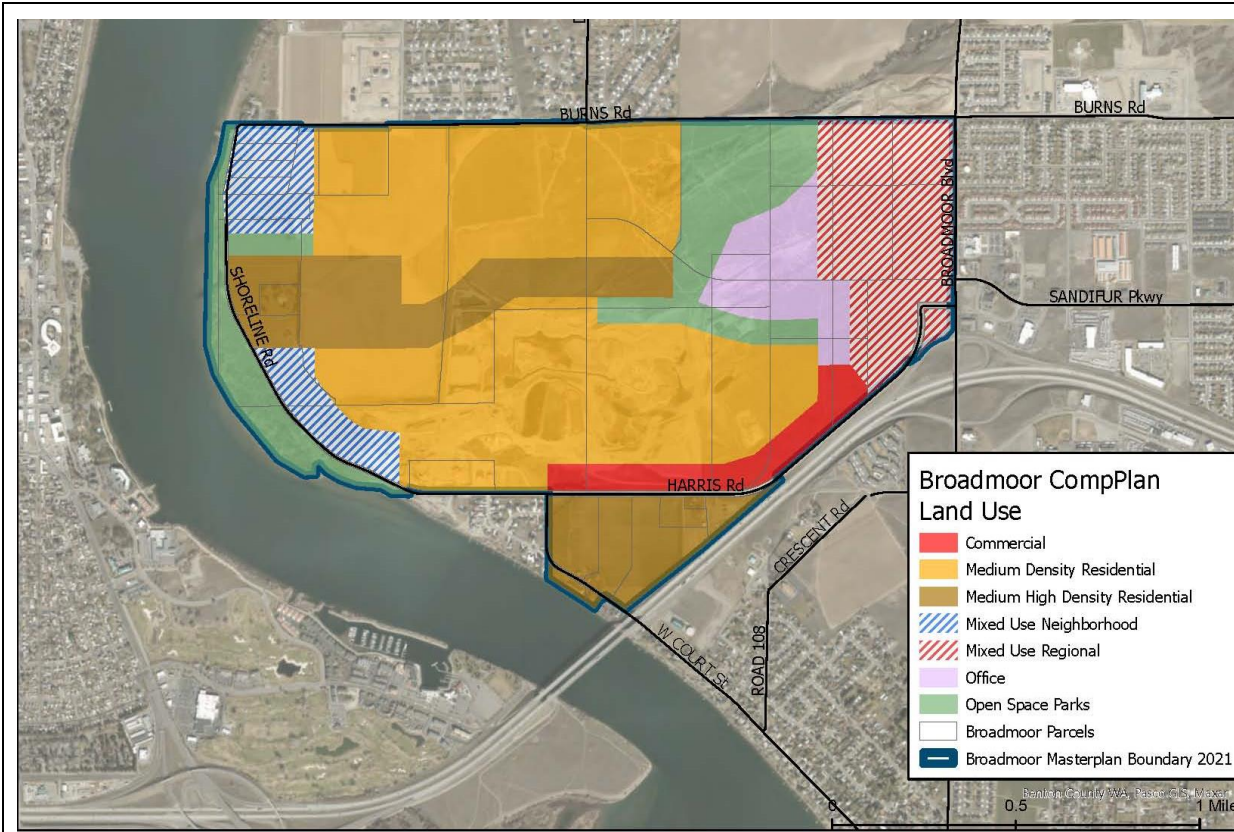


FIGURE 1-3: EXISTING COMPREHENSIVE PLAN LAND USE

TABLE 1-1: COMPREHENSIVE PLAN LAND USES (EXISTING)

Comprehensive Plan Land Use	Area (Acres)	Percent
Medium Density Residential	549	44%
Medium - High Density Residential	201	16%
Office	67	5%
Commercial	50	4%
Mixed-Use Neighborhood	76	6%
Mixed-Use Regional	139	11%
Open Space	159	13%
<b>Total</b>	<b>1,240</b>	<b>100%</b>

\*Land use does not include rights-of-way (based on parcel geometry).

**Low Density Residential:** Although the land use does not include low density residential, an approved low density residential development (Solstice) will be developed in the Broadmoor area at a density of 2 to 5 dwelling units per acre.

**Medium Density Residential:** This allows single-family homes, townhouses, apartments, and condominiums at a density of 6 to 20 dwelling units per acre.

**Medium High Density Residential:** This allows single-family patio homes, townhouses, apartments, and condominiums at a density of 8 to 15 dwelling units per acre.

**Office:** This allows professional office and personal services, resource centers, etc.

**Commercial:** This land use includes neighborhood, community, and regional shopping and specialty centers, business parks, service, and office uses.

**Mixed-Use Neighborhood:** This allows townhouses, multi-family developments, neighborhood grocers/markets and drug stores, vertically integrated buildings, live-work spaces, and other neighborhood scale offices and uses.

**Mixed-Use Regional:** This allows general retail operations and shops, grocery stores, residential above commercial/office, high-density residential, dining, entertainment uses, etc.

**Open Space:** Development other than parks, trails, and recreational areas will be severely restricted.

### 1.2.3 ZONING

The Broadmoor area is currently zoned R-T (Residential Transition), C-1 (Retail Business), R-1/PUD (Low Density Residential Planned Unit Development), and R-3 (Medium Density Residential). The R-T zone is generally applied as a holding zone for areas that lack necessary urban utility services. As utilities become available, lands zoned R-T can then be transitioned (through the rezone process) to a zoning classification in conformance with Comprehensive Plan designations for the affected lands.

### 1.2.4 TOPOGRAPHY AND STORM DRAINAGE

The topological features at the Broadmoor area include dunes, gradually sloping or flat agricultural areas and residential development, man-made below-water table depressions at the American Rock Products (ARP) facility, and a steep sloped hillside, running north-south, located north of Burns Road outside of the Broadmoor area.

The City's stormwater runoff system works through a system of catch basins, infiltration galleries, collection ponds, on-site collection and dissipation systems, and grassy swales along roadways. The Broadmoor area currently does not have a storm drainage system in place in the undeveloped areas. Figure 1-4 indicates existing topography of the area.

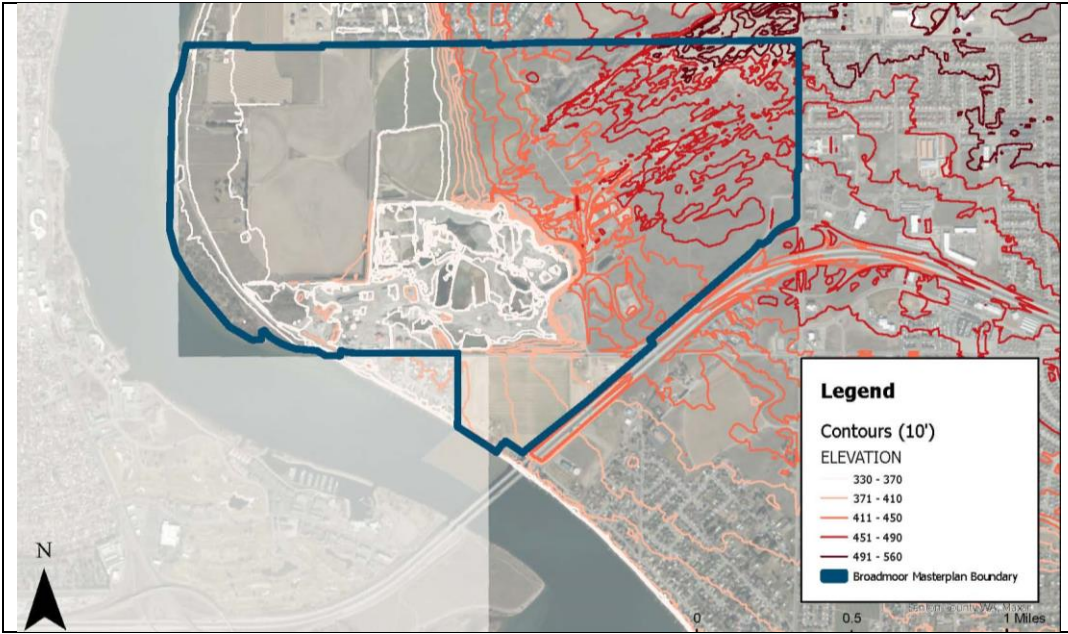


FIGURE 1-4: TOPOGRAPHY

### 1.2.5 OWNERSHIP

Land in this area is mostly under private ownership. There are five large parcels with parcel sizes more than 100 acres. Pasco School District owns approximately 74 acres of land north of this area. Figure 1-5 shows ownership of the area as of 2021. Most of the large parcel owners have expressed interest for a planned development of the area.

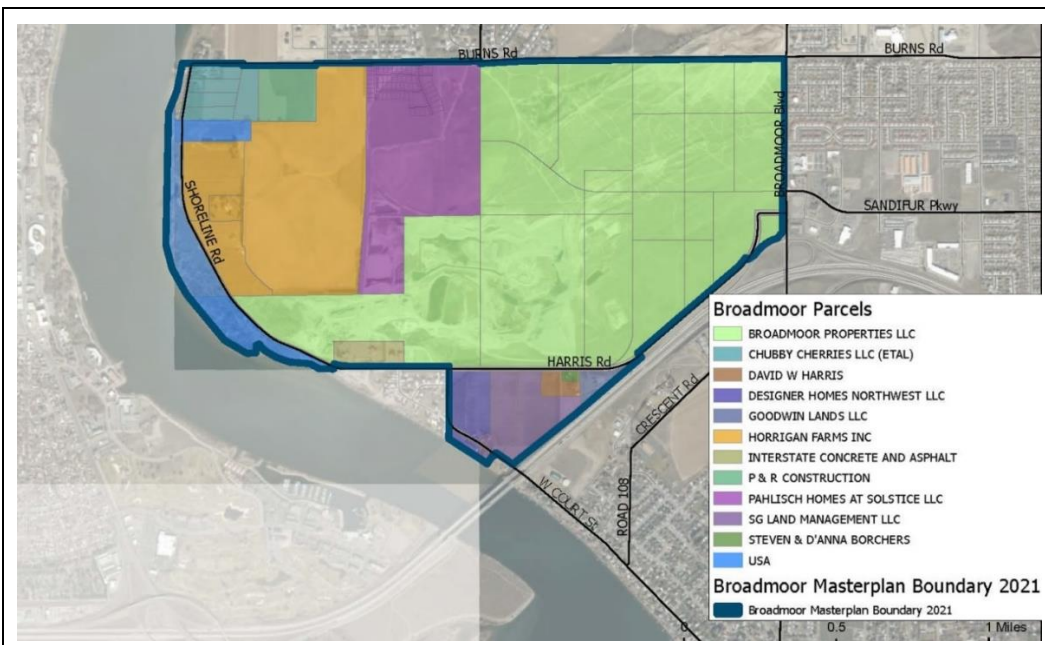


FIGURE 1-5: LAND OWNERSHIP

### 1.2.6 ACCESS AND CIRCULATION

The I-182/Road 100 (Broadmoor Boulevard) Interchange is located directly southeast of the Broadmoor area. This interchange provides a convenient connection point to all areas within the Tri-Cities and is one of the few main connections between Pasco and I-182. The interchange was upgraded in 2009 with the addition of two new ramps to facilitate easier access to the freeway from Broadmoor Boulevard.

City and County streets are located on the periphery of the Broadmoor area only. Broadmoor Boulevard is located to the east, Harris Road is to the south, Shoreline Road is to the west, and Burns Road is to the north. Dent Road and Kohler Road extend to the northern boundary of the Broadmoor area intersecting Burns Road. Burns Road runs east west and extends beyond the boundary of the Broadmoor area. Sandifur Parkway from the east ends at Broadmoor Boulevard. Except for Broadmoor Boulevard, all roads within the area have two lanes. This area lacks adequate internal circulation, in terms of north-south connection between Burns Road and Harris Road, and east-west connection between Broadmoor Boulevard and Shoreline Road.

### 1.2.7 INFRASTRUCTURE

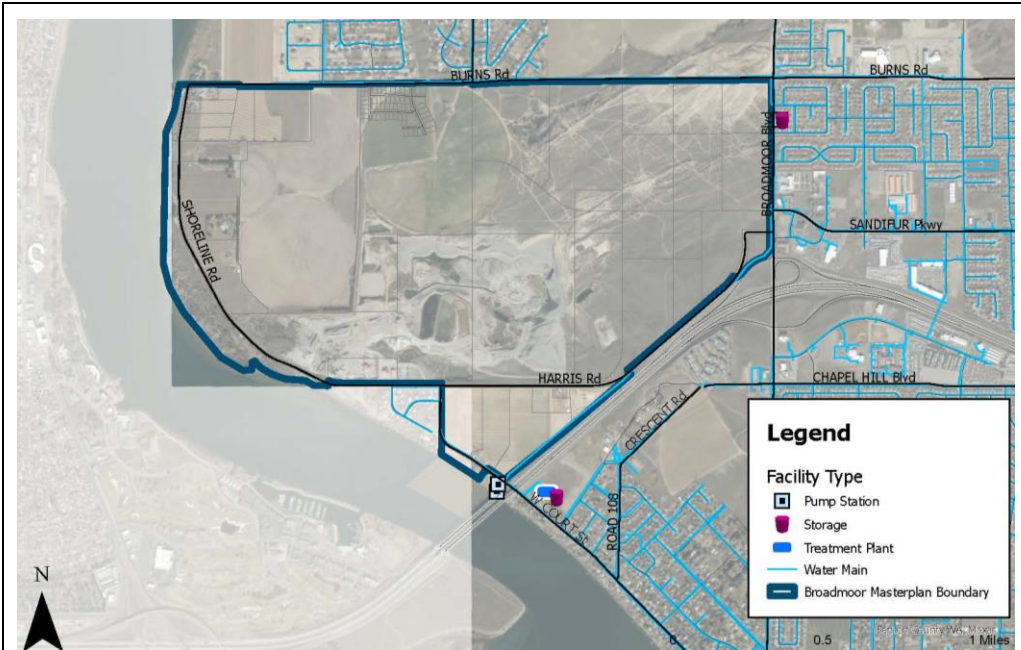
As discussed previously, the Broadmoor area currently does not have adequate infrastructure available to serve the area. The new subdivisions south of Burns Road (Solstice), and the planned unit development (Columbia Shores) south of Harris Road, are served by the City's water system and soon will be served by the City's sewer system. Proposed developments are extending service internal of their development.

#### **WATER**

The City's water system is supplied from surface water withdrawals from the McNary Pool of the Columbia River. Currently, the system is served by two surface water treatment plants, Butterfield Water Treatment Plant (WTP), which is a conventional filtration plant, and West Pasco WTP which is an ultrafiltration membrane plant. According to the 2019 Water System Plan (WSP), the Butterfield WTP has capacity of 26.8 million gallons per day (mgd) while the West Pasco WTP has a capacity of 6.0 mgd with the ability to expand to 18.0 mgd. The West Pasco WTP is located on the southern edge of the Broadmoor area. An existing storage tank is located on Broadmoor Boulevard.

Figure 1-6 indicates major water infrastructure serving the Broadmoor area.

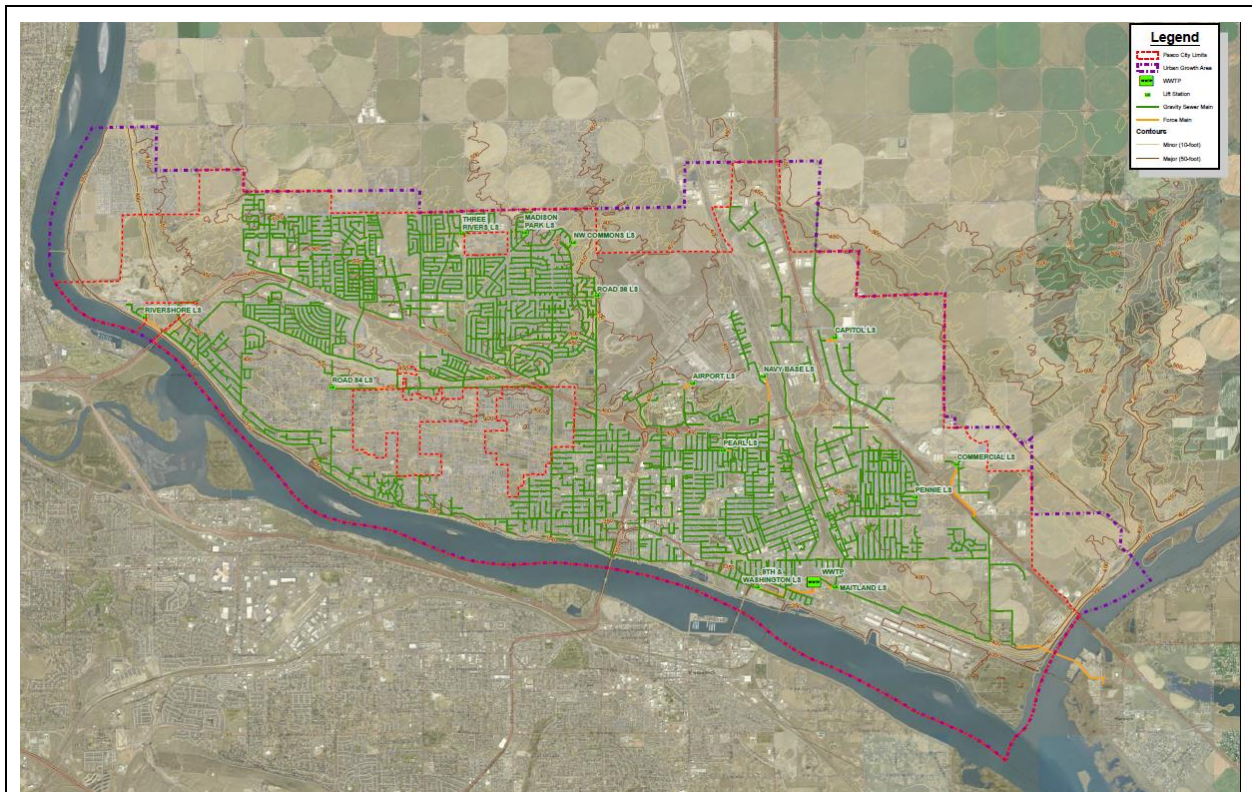




**FIGURE 1-6: WATER INFRASTRUCTURE**

## SEWER

Figure 1-7 shows the existing sewer system for the entire City as part of the 2021 Comprehensive Sewer Plan Addendum, completed in November of 2021.



**FIGURE 1-7: PASCO SEWER PLAN ADDENDUM**

The Harris Road Sewer Transmission Main, a 30-inch-diameter pipe, has recently been constructed and will provide sewer service in portion of the Broadmoor area. The new main runs from the West Pasco Trunk Sewer at the intersection of Road 111 and Court Street to the north crossing under I-182, running northeast along Harris Road at the project boundary. Additional sewer trunk lines and a pressure lift station have been constructed through a Local Improvement District (LID).

### ***ELECTRICITY***

Electrical service in the Broadmoor area is split between the Franklin County PUD and the Big Bend Electrical Cooperative. The Franklin County PUD serves the Central Pre-Mix facilities and all properties west of the Wilson farm. The remainder of the area is served by Big Bend. The City is working with these agencies regarding future growth in this area.

### ***GAS***

Gas service is available in the area near the intersection of Sandifur Parkway and Broadmoor Boulevard. The residential subdivisions to the east of Broadmoor Boulevard and the commercial properties extending down Sandifur Parkway benefit from this service.

## **1.2.8 PARKS AND OPEN SPACE**

The City of Pasco Parks and Recreation Department provides physical facilities and operates a variety of programs to meet the community's needs. This includes establishing a neighborhood park system and other park facilities, providing organized recreation programs for all age groups, and performing maintenance to maximize the service life of existing parks and recreation facilities. The City sets aside budget that supports the acquisition, development, and maintenance of facilities, and operation and management of recreational programs. Since most of the Broadmoor area is undeveloped, there is no park land currently identified in this area. The City's Parks, Recreation and Open Space Plan aims to ensure equitable park access. The City uses a ¼-mile walking distance from park access points.

The federally owned property between the shoreline and Shoreline Road is a designated open space. The area is also identified as a critical area geologically and for habitat purposes.

The tailwater pond south of Burns Road was part of the South Columbia Basin irrigation system. Portion of the tailwater pond and ditch leading to the pond are designated open space. These are no longer operational due to a recent change in irrigation water management and will no longer remain open space.

## **1.2.9 WILDLIFE AND HABITATS**

The Washington Department of Fish and Wildlife (WDFW) identifies and defines Priority Habitats and Species (PHS) to prioritize conservation of important fish, wildlife, and habitat resources in



Washington state. Currently in the Broadmoor area there are State Candidate species including the black-tailed jackrabbit, burrowing owl, sagebrush sparrow, and sagebrush lizard. The shrub-steppe habitat, which currently encompasses a large portion of the Broadmoor area, is classified as priority habitat as shown in Figure 1-8.



**FIGURE 1-8: PRIORITY HABITATS AND SPECIES**

Table 1-2 indicates priority species in the Broadmoor area:

**TABLE 1-2: PRIORITY SPECIES**

Common Name	Scientific Name	Animal Type	Species Status	
			State	Federal
Black-tailed jackrabbit	<i>Lepus californicus</i>	Mammal	SC	none
Burrowing owl	<i>Athene cunicularia</i>	Bird	SC	none
Sagebrush sparrow	<i>Artemisiospiza nevadensis</i>	Bird	SC	none
Sagebrush lizard	<i>Sceloporus graciosus</i>	Reptile	SC	none

Notes:

\*Ord's Kangaroo Rat is a Washington State Monitored Species and is listed on the International Union for Conservation of Nature (IUCN) Red List of Threatened Species as Least Concern.

SC = State Candidate

(Sources: WDFW, 2018a; Cassola, 2016)

## 1.3 ASSETS AND CONSTRAINTS

### 1.3.1 ASSETS AND OPPORTUNITIES

**Proximity to the freeway and the river.** The Broadmoor area has easy access to I-182. This would provide an opportunity for the area to connect to the rest of the City and the region. The location of the area abutting the Columbia River provides significant recreational opportunities.

**Area within the City limits and urban growth area (UGA).** The location of the Broadmoor area mostly within the City's UGA allows the City to invest in capital facilities and promote growth at urban density.

**Surrounding growth.** Developments are already occurring in the vicinity, especially to the east and north of the Broadmoor area, and within the Broadmoor area (Solstice, Dunes, Columbia shores, affinity). Medium-high density growth expected in the area will support future developments. Additionally, there is interest in commercial developments in the Broadmoor area.

**Vacant land ownership.** There are several landowners who own parcels larger than 10 acres. There are several parcels ranging from 100 acres to 500 acres. Large portion of vacant lands provide opportunities for planned developments in the area.

**Open space.** The shoreline area west of Shoreline Road is publicly owned and a designated critical area. This provides an opportunity for preservation of the area, low intensity recreation, and shoreline public access.

**Tailwater pond.** The location of the pond and ditch present challenges for laying out lots and streets. However, this drainage has recently been re-routed to the Columbia River, providing better flexibility for developing the area.

- Proximity to the freeway and the river
- Area within the City limits and urban growth area (UGA)
- Surrounding growth
- Vacant land ownership
- Open space
- Rerouted irrigation

### 1.3.2 CONSTRAINTS

**Limited access.** Although the Broadmoor area is in close proximity to the freeway, it currently lacks adequate local access roads to serve the area. Most of the roads to and from the area are not constructed to the City standards for road design or capacity. Regional access and access to the Broadmoor area both provided by Broadmoor Boulevard.

**Limited infrastructure.** The Broadmoor area lacks adequate infrastructure, especially water and sewer services.

**Gravel operation.** A portion of the Broadmoor area is currently being used for gravel mining. The process of gravel mining and the production of ready-mix concrete creates the potential of nuisance conditions related to dust, noise, and truck traffic. Once the operation ends, it may take years for reclamation of the gravel mining site. In addition, steep slopes created in the gravel mine area will make construction of streets challenging.

- Limited access
- Gravel operation
- Truck traffic
- Wildlife habitat

**Truck traffic.** Due to the Central Pre-Mix operations and, to a lesser extent, nearby farming, a high percentage of traffic on Harris Road is truck traffic. The truck traffic creates potential conflicts for future non-industrial traffic.

**Wildlife habitat.** The existing shrub-steppe habitat, which currently encompasses a large portion of the Broadmoor area, is classified as a state priority habitat. Adverse impacts on priority habitats should be avoided, minimized, or mitigated according to the City's Critical Areas Ordinance.

## 2 BROADMOOR VISION AND PLAN

### You will find in this Section:

- Planning vision
- Plan elements
  - Land use
  - Community centers and corridors
  - Access and circulation
  - Infrastructure
  - Wildlife and habitats
  - Parks and open space
  - Storm drainage

## 2.1 PLANNING VISION

The Broadmoor area is intended to be developed as a well-connected and sustainable community for diverse income and age groups. The area will provide the community with various choices to live, work, shop, and recreate in a pedestrian- and transit-friendly environment.

*... a well-connected and sustainable community... will provide the community with various choices to live, work, shop, and recreate in a pedestrian- and transit-friendly environment.*

### 2.1.1 PLANNING PRINCIPLES

#### 1. Encourage a pedestrian- and transit-friendly environment.

Developments shall implement development patterns that promote walkability.

#### 2. Establish a connected community with ample choices in the circulation network.

Create an efficient bike, pedestrian and vehicular circulation network within the area and with connections to the rest of the City. A public transportation system to be utilized to connect residential and commercial land use districts with each other as well as with the region.

#### 3. Provide a variety of housing choices with a priority on higher density developments.

The area should include a wide range of housing types, such as single-family homes, townhomes, condominiums, apartments, etc., that can meet the demands and preferences of current and future residents.

#### 4. Create economic opportunities for all income groups.

Provide flexibility and choices for a variety of small businesses, entrepreneurship, and general economic growth. Promote businesses in commercial, non-residential, and mixed-use areas to improve the economic vitality of the City.

#### 5. Promote environmental protection and a network of open spaces.

The area should be connected by open spaces, greenbelts, recreational facilities (trails, pathways), and roadways.

#### 6. Develop an aesthetically pleasing community with quality design.

The design guidelines will promote a complementary scale and size of neighborhood blocks, building form, and landscape design. It will create a balance between residential and commercial uses that will promote sustainable living and working conditions.

## 7. Provide infrastructure and public facilities to meet the need of the residents.

The area will have necessary infrastructure to support growth and ensure efficient use of public facilities. This area should also provide amenities and services as growth takes place, such as schools, parks, police and fire services, and recreational opportunities.

## 8. Offer flexibility and creativity with monitoring

The design and development will encourage creativity. In order to promote creative design and a walkable community, there will be provisions of choices and flexibility in the development. Development of the area will be monitored on a periodic basis to meet the planning vision of the Broadmoor area.

## 2.2 PLAN ELEMENTS

This section lays out the plan for future growth within the Broadmoor area. The area is envisioned to be developed as a mixed-use pedestrian-friendly community with a variety of jobs and housing opportunities to meet the needs of the residents. Land uses in this Plan is proposed to be updated from the existing Comprehensive Plan land uses. Land use, circulation, infrastructure, open space, and other public amenities are planned in an integrated and consistent manner. Various land uses and projected densities are planned with adequate public facilities and infrastructure to support them. This Plan accounts for the projected population in the Broadmoor area as part of the City's 20 years population projection in the 2018-2038 Comprehensive Plan.

*Land use, circulation, infrastructure, open space, and other public amenities are planned in an integrated and consistent manner.*

### 2.2.1 LAND USE

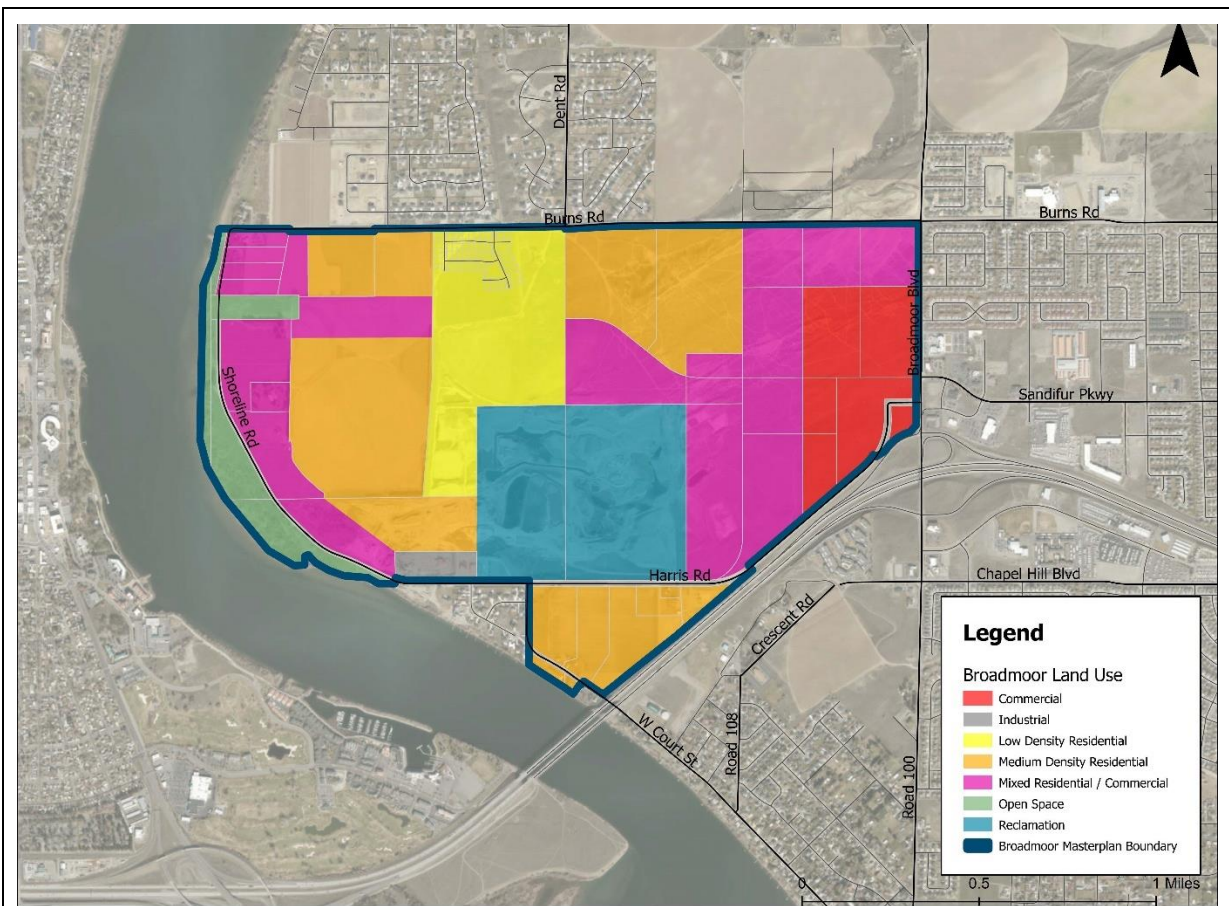
The Broadmoor area is envisioned to become a mixed-use and diverse community with easy access and circulation for bike, pedestrian and vehicular traffic. The area is also expected to accommodate a substantial portion of the City's future growth in a higher density environment. It is expected that it will have necessary uses and amenities to serve the area such as shopping centers with commercial land use, single- and multi-family housing, civic structures, and various parks. This will also continue existing industrial use until 2035. After this time period, a land use amendment will follow with an intended Medium Density Residential use in this land. Examples of specific uses include low, medium and high-density housing; offices; banks; grocery; retail shops; health/fitness; auto services; library; schools; recreational facilities (public and/or private); fast-food restaurants; sit-down restaurants; public open space; and recreational parks, pathways, and trails.



The uses can be accommodated in various land uses as follows:

- Low Density Residential
- Medium Density Residential
- Mixed Residential and Commercial
- Commercial
- Open Space
- Reclamation
- Industrial

Land uses are shown in Figure 2-1. Table 2-1 indicates various land use areas in Broadmoor.



**FIGURE 2-1: PROPOSED LAND USE IN BROADMOOR**

**ABLE 2-1: LAND USES IN THE BROADMOOR AREA**

<b>Land Use</b>	<b>Area (Acres)</b>	<b>Percent</b>
Low Density Residential	144	12%
Medium Density Residential	361	29%
Commercial	104	8%
Mixed Commercial Residential	370	30%
Open Space	61	5%
Reclamation	187	15%
Industrial	14	1%
<b>Total</b>	<b>1,240</b>	<b>100%</b>

**LOW DENSITY RESIDENTIAL**

The intent of Low Density Residential land use is to allow single-family detached and/or attached residential homes at a density of 2 to 5 dwelling units per acre. Single-family housing developments currently exist north of Burns Road bordering the Broadmoor area. Within the Broadmoor area, low density residential development (Solstice) was previously permitted and is located south of Burns Road. Developments are guided by the City's existing R-1 zoning standards.

**MEDIUM DENSITY RESIDENTIAL**

The intent of Medium Density Residential land use is to allow small lot single-family homes, both attached and detached, as well as duplexes, multiplexes in this land use. This also includes cottage housing, multi-family homes, apartments, townhomes, and condominiums. Medium density land use is proposed south of Burns Road and south of Harris Road. The density shall be at 6 to 29 dwelling units per acre.

**MIXED RESIDENTIAL AND COMMERCIAL**

The Mixed Residential and Commercial designation is generally located along the east and west sides within the Broadmoor area. This land use is also called Mixed-use in this document. Mixed-use land uses are intended to create a sense of community by increasing the interaction among different types of uses, such as residential, commercial, and office, and promoting pedestrian-friendly environments within neighborhoods. The purpose of mixed use is to encourage more efficient use of land and public services, increase economic return, reduce transportation costs, encourage human interaction, create place-making with day and evening activities, create a more sustainable development pattern, and increase development opportunities.

Mixed uses in this area should be compatible, supportive, and complement each other. Various uses should be placed in close proximity. To that effect, mixed use can be horizontal in a block setting, or vertical in a building setting. Uses include townhouses, multi-family developments, neighborhood grocery, drug stores, coffee/sandwich/ice cream shops, restaurants, breweries,

recreational facilities, vertically integrated buildings, live-work spaces, dry cleaner/laundromat, salons/spas, daycares, neighborhood-scale professional offices, medical/dental clinics, etc.

In this land use, a mix of use shall consist of at least 20% of residential development at a minimum density of 21 units per acre for the residential portion of the site. Maximum share of a single use in the mixed land use shall be 80%. Planning Director may approve ratios and/or density lower than the minimum up to 10% if the project meets the overall intent of the Mixed Residential and Commercial land use, and if the development proposal and density based on site plan indicate such need based on market research.

Mixed Residential and Commercial land use near the Columbia River is aimed to accommodate recreational uses along the river while maintaining the shoreline environments.

### ***COMMERCIAL***

The Commercial land use is intended to allow various commercial uses to serve the area including retail, wholesale, office, and business parks as well as appropriate public uses and government facilities.

### ***OPEN SPACE***

The Open Space land use is designated for areas that are publicly and/or privately owned to protect the natural features of the area and provide various recreational opportunities to the community while preserving the environmental features.

### ***RECLAMATION***

The Reclamation land use is designated for areas that are currently or more recently have been used for gravel mining operations and are planned for future reclamation. Various recreational and other appropriate uses can be designated in this area. However, a reclamation plan and future studies shall be required to designate appropriate uses in this area.

### ***INDUSTRIAL***

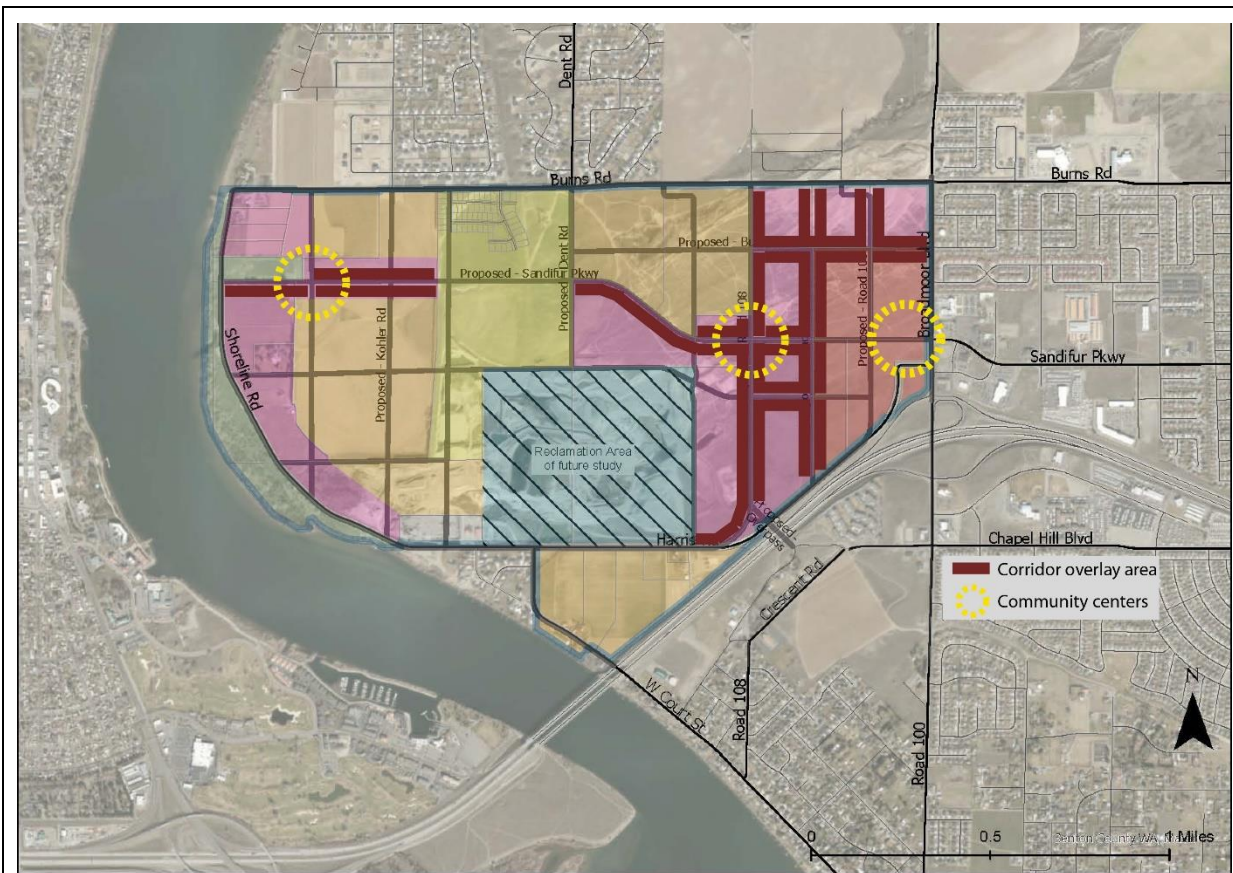
The intent of this land use is to allow short term operation of the existing concrete batch plant, equipment maintenance building and yard, office, and building material retailer. These uses will facilitate the development of the Broadmoor Area by providing essential construction and building materials in close proximity. This use will be terminated in 2035, and the land use will be amended to Medium Density Residential after the industrial use ends.

## 2.2.2 COMMUNITY CENTERS AND CORRIDORS

Community centers and street corridors are envisioned to be the activity centers of the Broadmoor area with a mix of activities for people to live, work, and recreate. Centers and corridors along with various land use standards will create and maintain a “sense of place” for the Broadmoor area. These centers are ideal to locate various public facilities such as transit centers, community and event centers, vertical mixed use, and anchor commercial businesses. Corridors are identified along the streets within Mixed Residential and Commercial land use and when this land use abuts Commercial land use. Corridors are ideal for mixed-use developments with an active street environment. Specific standards will apply to these centers and corridors, as mentioned in Section 3. Three locations have been identified in the Broadmoor area that shall serve as centers of various types. These are as follows (Figure 2-2):

*Community centers and street corridors ... will create and maintain a “sense of place” for the Broadmoor area.*

1. Gateway center at Broadmoor and Sandifur Parkway intersection
2. Mixed-use center at Road 108 and Sandifur Parkway intersection
3. Waterfront neighborhood center at the future north-south roadway and Sandifur Parkway intersection.



**FIGURE 2-2: CENTERS AND CORRIDORS IN BROADMOOR**



### 2.2.3 ACCESS AND CIRCULATION

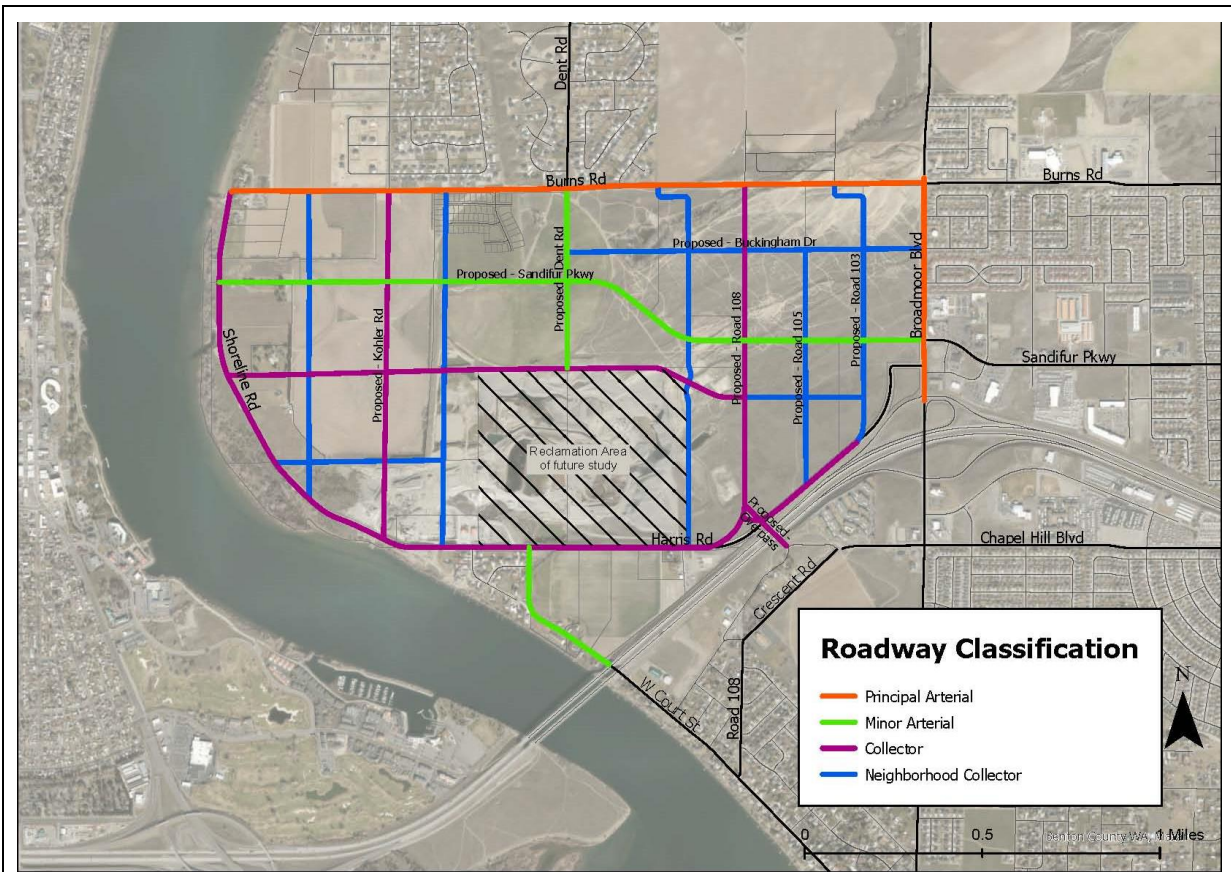
The area currently lacks adequate access. Several north-south and east-west arterial, collector and neighborhood collector roads are proposed with the intent of connecting with the existing street grids for better connectivity.

Existing Burns Road is proposed to be improved as a principal arterial and serve as a major east-west connection. Existing Broadmoor Boulevard is proposed to be improved as a principal arterial. Extension of Sandifur Parkway will be a central east-west corridor to serve as a minor arterial. Shoreline Road, Harris Road will be improved as collectors. Road 108, the extension of Kohler Road and an east-west road north of the gravel operation will be collectors. Buckingham Drive will continue east-west as a neighborhood collector. Road 103, Road 105, and additional not yet named roads will be neighborhood collectors as shown in Figure 2-3. Local access roads are not shown and will need to provide local access. Access and circulation requirements to and through the reclamation area will need to be determined in the future

*A "complete street" is a roadway that is designed to be safe, accessible, and functional for all users of the City's rights-of-way regardless of age or ability.*

An area abutting future extensions of Road 108 and Sandifur Parkway is being considered for future transit center.

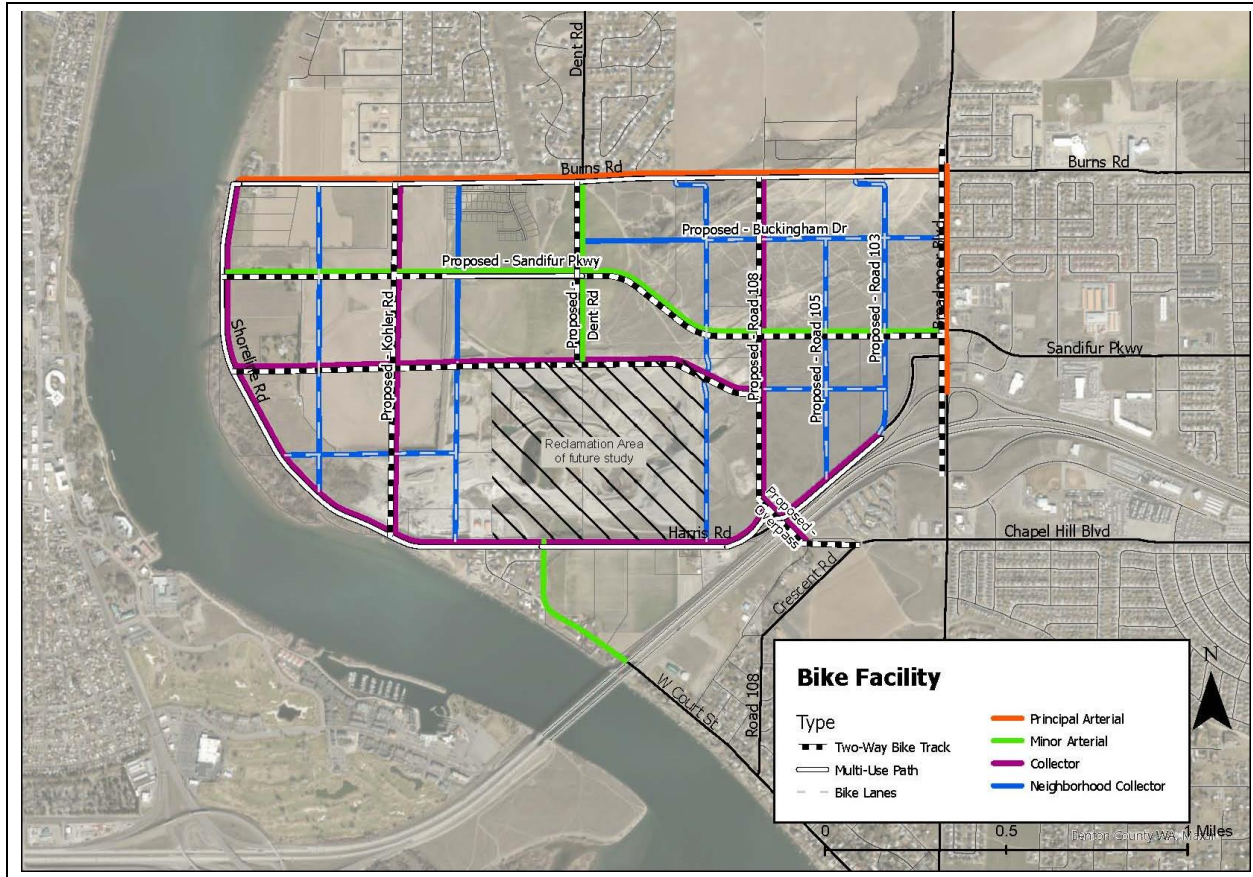
The Transportation System Master Plan (TSMP) provides lists of specific street categories and design standards based on traffic demand, and the character of the land use district they will serve. Figure 2-3 indicates the proposed major road network and how the local road network shall be arranged. Detailed standards are also identified in Section 3 under each land use, as well as in Section 3.6, Street Environment and Public Realm.



**FIGURE 2-3: PROPOSED MAJOR ROAD NETWORK**

Pedestrian and bike facilities are required to comply with the city’s complete streets ordinance. A “complete street” is a roadway that is designed to be safe, accessible, and functional for all users of the City’s rights-of-way regardless of age or ability. The vision of the master plan is to provide enhanced walkability and multi-modal access. The major road network will include enhanced bike as shown in Figure 2-4. Additionally, pedestrian connections and public access to the shoreline area will be provided at multiple locations. The Pasco Rivershore Linkage and Amenity Plan prepared in 2012 indicates potential trails along the shoreline. This plan recommends working with the U.S. Army Corps of Engineers, the County, and property owners to design and build an extension of the Sacagawea Trail with raised wildlife watching decks on piers. Additional access along the shoreline shall be provided at locations that maximize non-motorized public access to shoreline such as, the road ends of Burns Road and along Shoreline Road. See Figure 2-8 for proposed park along the shoreline area and trail connections. The City will work with the property owners to explore realigning Shoreline Road to increase public access to the shoreline for recreational purposes, and allow future developments in this area. Further studies and detailed planning will be needed for appropriate developments along Shoreline Road to create a shoreline-based mixed-use community. Figure 2-4 indicates the pedestrian and bike network proposed in conjunction with the street network. Future local roads will be developed with the pedestrian network.





**FIGURE 2-4: PROPOSED ENHANCED BIKE NETWORK**

The freight transportation route in the Broadmoor area includes a segment of the Broadmoor Boulevard from I-182 to Burns Road, and Burns Road to Dent Road. See Figure 2-5.

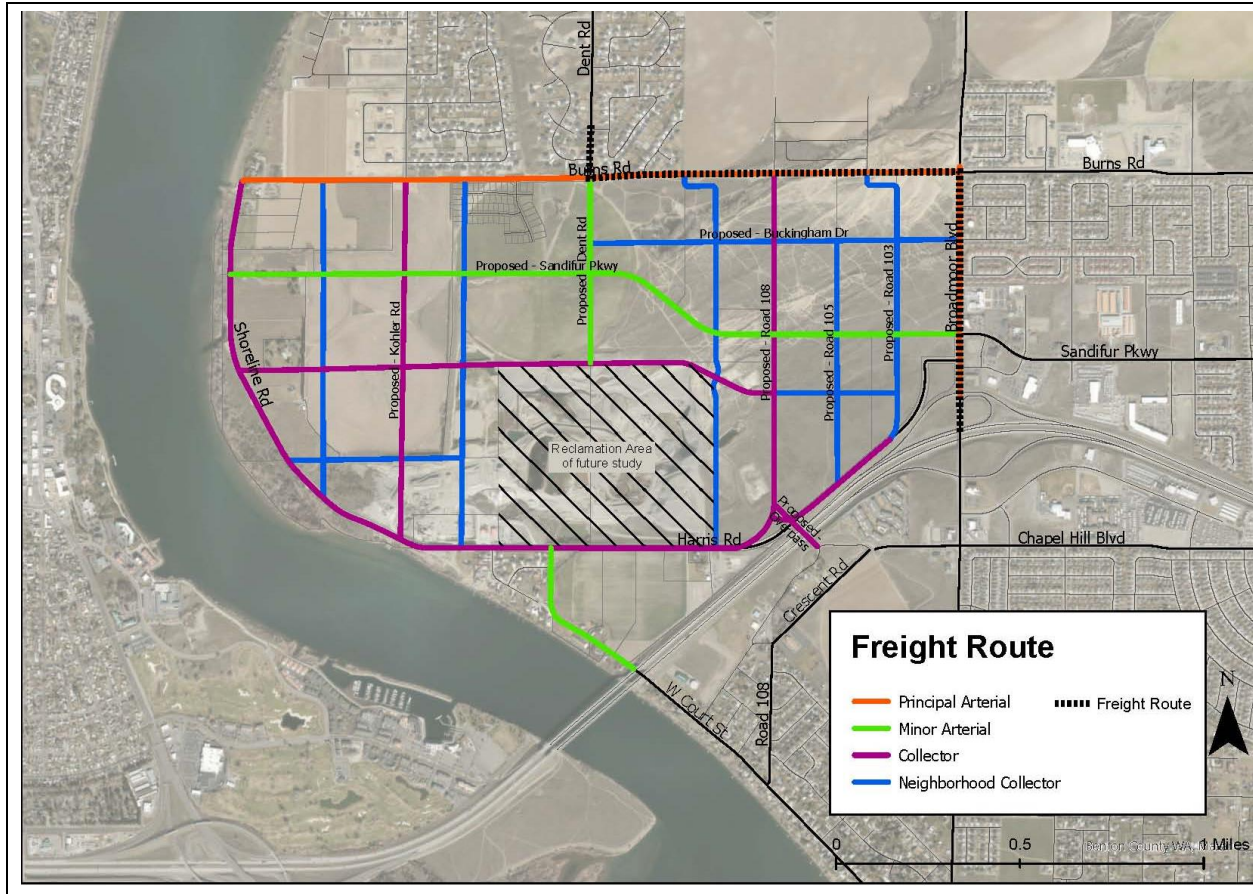


FIGURE 2-5: FREIGHT TRANSPORTATION ROUTE

## 2.2.4 INFRASTRUCTURE

The City is establishing a Local Improvement District (LID) and assessing the newly available Tax Increment Financing (TIF) for the Broadmoor area to help fund certain infrastructure improvements. An LID is a method by which property owners can share in the cost of infrastructure improvements. LIDs can be used to help finance improvements such as street construction and improvements, utility expansion, streetscape improvements, park construction and improvements, revitalization, etc. The TIF will allow the City infrastructure financing options by using the incremental property tax increase within the area.

*The TIF will allow the City infrastructure financing options.*

### WATER

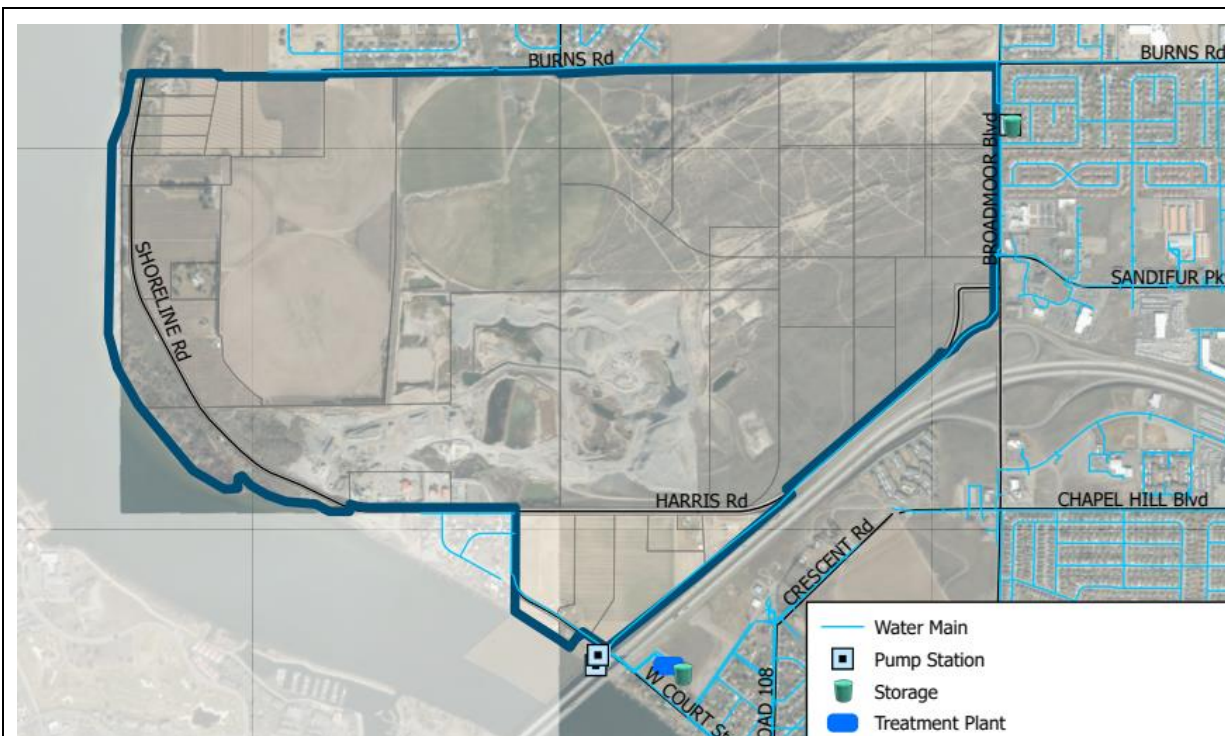
The area will be served through expansion of existing services along with capital projects to increase capacity. The City's capital projects planned to serve the Broadmoor area include:

1. West Pasco WTP: capacity of 6.0 million gallons per day (modular/expandable; the build-out capacity is 18 million gallons per day when all six treatment trains are installed)



2. Broadmoor Boulevard reservoir (storage): 1 million gallons
3. Pump stations near I-82

New water mains are proposed along Court Street, Harris Road, and Shoreline Road to the south. Additional mains are proposed in the area between Harris Road, Shoreline Road, and Burns Road. Figure 2-6 indicates major water infrastructure in Broadmoor area.



**FIGURE 2-6: MAJOR WATER INFRASTRUCTURE**

## SEWER

Two lift stations have been proposed for this area, one on Kohler Road, north of the area, and the other one on the south side, close to the existing gravel operation. Three major north-south trunk lines are proposed in the area as of March 2022 as shown in Figure 2-7. The City plans to provide sewer services to the areas that are currently not served through the LID process according to the City's Comprehensive Sewer Plan Addendum, also shown in Figure 1-7.



**FIGURE 2-7: MAJOR SEWER INFRASTRUCTURE (PROPOSED)**

## 2.2.5 WILDLIFE AND HABITATS, PARKS AND OPEN SPACE

Development in areas where PHS are present will require mitigation for any unavoidable adverse impacts. The City, property owners, and WDFW are working on a mitigation plan to be adopted by all parties. See Broadmoor Draft Environmental Impact Statement (DEIS) for details (Appendix A).

Parks and open space in the Broadmoor area will be added in parks, plazas, and trails. The City's 2022 draft Parks, Recreation and Open Space (PROS) Plan indicates level of service (LOS) standards for various parks and recreational facilities. The LOS goal is 2 acres of neighborhood parks and 2.1 acres of community parks per 1,000 population. Based on the existing inventory, the City is currently providing 1.3 acres of neighborhood parks and 1.4 acres of community parks per 1,000, which is around 70% of its goal. According to the City of Pasco's 2018-2038 Comprehensive Plan, Broadmoor area needs to accommodate approximately 22,000 people. To achieve the LOS goal for future population, the City will need approximately 44 acres of land for neighborhood parks and approximately 46 acres of land for community parks in the Broadmoor area (Table 2-2).

TABLE 2-2: PARKS AND OPEN SPACE REQUIRED IN THE BROADMOOR AREA

Park/ Open Space Type	Area (Acres)
Neighborhood parks	44
Community parks	46
<b>Total required</b>	<b>90</b>
Note: LOS 2 acres of neighborhood parks and 2.1 acres of community parks per 1,000 population; parks and open space for a total 22,000 future residents in the Broadmoor area.	

Open space throughout the area shall be provided during the developments of the area to create a network of open space, parks, and bike and pedestrian trails. They will be in the form of neighborhood and community parks, street corner plazas, courtyards, trails, streetscape, greenbelts etc. A greenbelt is proposed along the current gravel mining operation site which is expected to be reclaimed after the operation ends in 2025. See Figures 2-9 and 2-10.

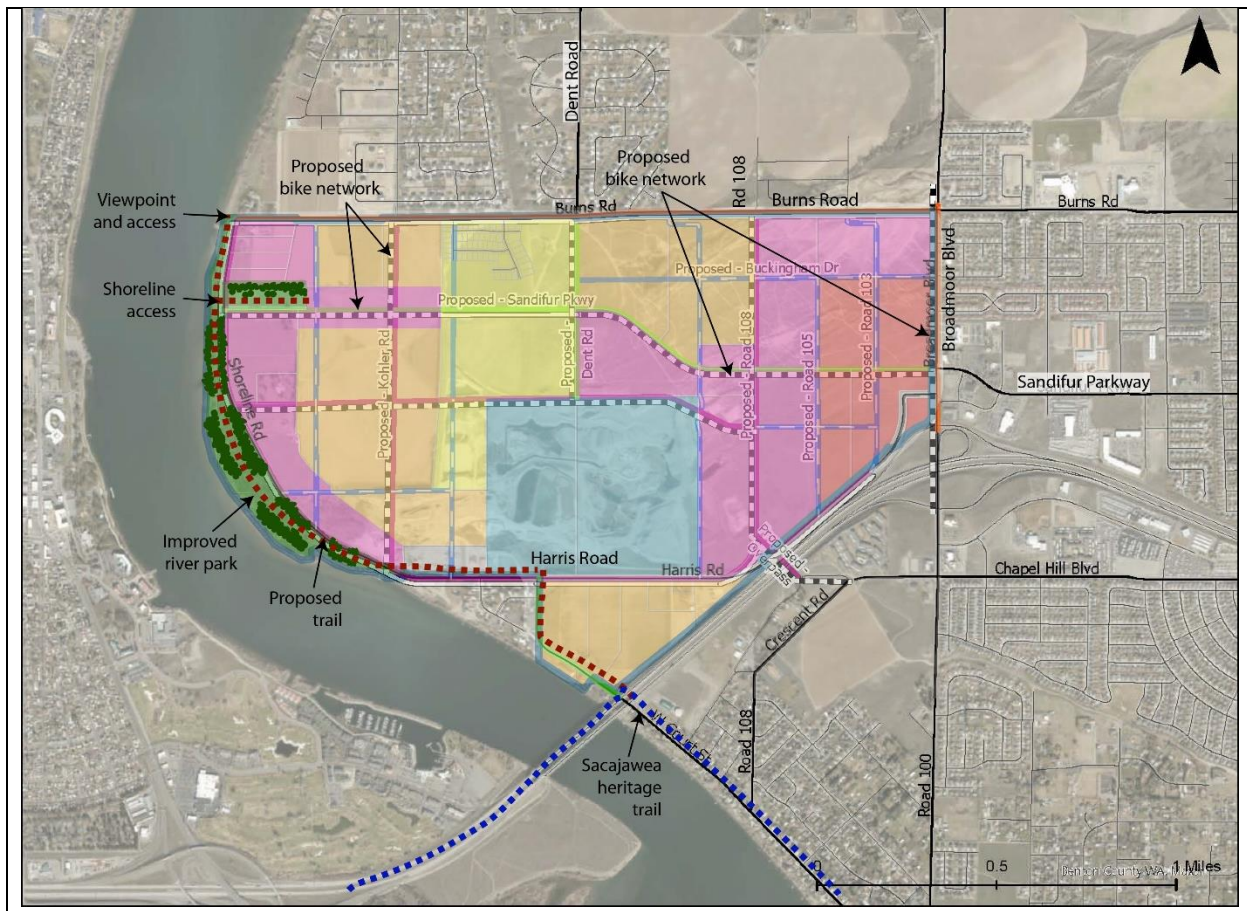
To ensure equitable park access, the planning team uses a ¼-mile walking distance from park access points. The 2018-2038 Comprehensive Plan also identifies approximately 40 acres of parkland to be designate in the City's UGA. A significant portion of that parkland will be in the Broadmoor area. No specific location has yet been identified for park lands. Additionally, the existing mining operation area will be reclaimed to offer some open space and recreational uses.

*... developments ...to create a network of open space, parks, and bike and pedestrian trails.*

The federally owned property on the west side of the Broadmoor area contains approximately 61 acres of land. This existing natural open space along the shoreline will continue to serve the Broadmoor area with shoreline public access and open space. Hiking and walking trails through this area would provide for public access, recreation, and public enjoyment of the river. Additional improvements in this area are discussed in The Pasco Rivershore Linkage and Amenity Plan (Rivershore Linkage Plan) prepared in 2012, and under sub-section 2.2.3 Access and Circulation.

Figure 2-8 indicates proposed park and trail facilities on the federally owned property. It also includes proposed bike facilities throughout the Broadmoor area. Proposed trail along the shoreline is intended to connect with the Sacajawea Heritage trail. The following short and long terms plans are proposed in the federally owned area according to the Rivershore Linkage Plan.





**FIGURE 2-8: PARKS AND TRAIL (PROPOSED)**

### Short term plans

1. Work with Army Corps to design and build a continuation of the Sacagawea Trail along the river to City Limits line. Work with County and property owners to design and build a continuation of the Sacagawea Trail to the northernmost Urban Growth Boundary line.
2. Include bona fide river access points/future parks along this section of trail which connect to Shoreline Road.
3. Begin negotiations with Army Corps to establish park facilities/raised view decks along river.
4. With Property owner approval (and covenant) Begin irrigation and tree planting along proposed greenbelt.

### Long term plans

5. Improve area for waterfowl and elevated bird watching along the rivershore trail, adding nature trails as needed.
6. Greenbelt/park/pocket wildlife preserve areas along river and along main boulevard;



7. Boat basin and launch; Possible restricted, non-motorized only boating area near wildlife reserve area.
8. Rivershore commercial development scaled to pedestrian/bicycle traffic.



**FIGURE 2-9: PARKS, TRAILS, AND OPEN SPACE (PROPOSED)**



FIGURE 2-10: PARKS, TRAILS, AND OPEN SPACE WITH PROPOSED LAND USE

## 2.2.6 STORM DRAINAGE

All new developments and redevelopments in the City are required to design and install stormwater systems that retain runoff on site for the established design storm event. On-site stormwater collection shall be implemented by projects and may utilize on-site infiltration galleries or collection swales. These shall function such that stormwater does not leave the project or development, the system functions effectively, and the project avoids impacts to the Columbia River or native wetlands or riparian areas. In addition to addressing storm drainage at the project level, the City will continue to require storm water treatment systems for all road construction.

## 3 DEVELOPMENT STANDARDS

### **You will find in this Section:**

- Purpose and applicability
- Urban block standards
- Building types and standards
- Land use standards
- Community centers and corridor standards
- Street environment and public realm
- Parking standards
- Landscaping standards
- Environmental protection



### 3.1 PURPOSE AND APPLICABILITY

The purpose of the development standards is to implement the Broadmoor Master Plan and the vision, goals and policies of the Comprehensive Plan for the Broadmoor area.

Standards in this section will be applicable to the entire Broadmoor area. These standards shall be considered a part of the Pasco Municipal Code (PMC). Additional sections of the PMC such as individual zoning code standards shall apply. The residential design standards approved under the PMC shall also apply. When there is a conflict, the Broadmoor development standards shall apply. The Planning Director or her/his designee shall review and may make exceptions when certain standards, or deviation from these standards will better implement the vision of the Broadmoor area. Table 3-1 indicates the land use designations within Broadmoor area and associated zoning.

*... implement the Broadmoor Master Plan and the vision, goals and policies of the Comprehensive Plan for the Broadmoor area.*

In the development regulations, “shall” is used to reflect mandatory requirements, and “should” allows City staff more flexibility to approve an alternative option if the Planning Director or her/his designee finds that it meets the overall intent of the Broadmoor area.

**TABLE 3-1: LAND USE AND ZONING**

Broadmoor Land Use Designation	Zoning
Low Density Residential	R-1
Medium Density Residential	R-2, R-3, R-4
Mixed Residential & Commercial	Mixed Residential & Commercial, C-1 <sup>1</sup> , R-3 <sup>1</sup> , R-4 <sup>1</sup>
Commercial	C-1, C-2
Open Space	Open Space
Reclamation	RT
1. Allowed only when they comply with Mixed Residential and Commercial development standards as part of a mixed-use site plan.	

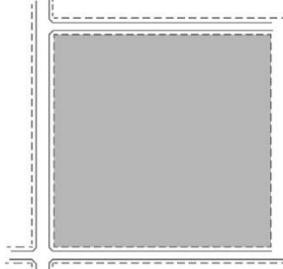
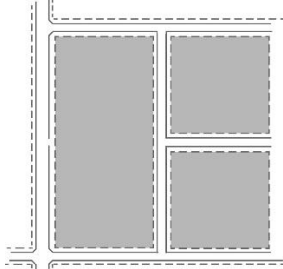
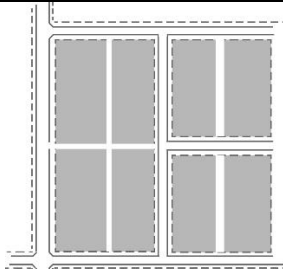
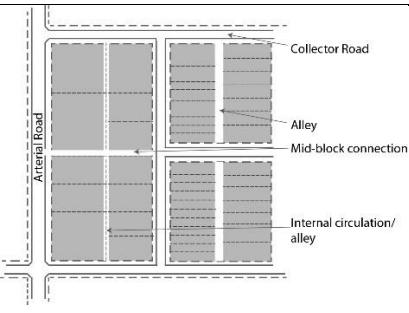
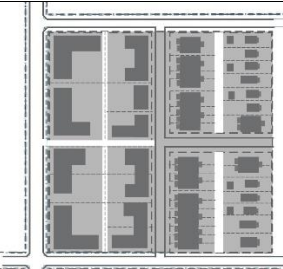
### 3.2 URBAN BLOCKS

#### 3.2.1 INTENT

The intent of urban blocks standards is to ensure that block sizes and layouts promote efficient connectivity. The interconnected and gridded layout of streets and smaller blocks promotes better accessibility and a pedestrian-friendly environment.

### 3.2.2 BLOCK STANDARDS

1. Blocks shall be designed and subdivided to create a network of circulation as shown in Figure 3-1 and according to the block dimension standards in Table 3-2.

	<p>Step 1: Large lots (5 acres or more) shall be subdivided further or developed with an internal road network as required by the City to create a highly connected, accessible development.</p>
	<p>Step 2: Additional streets are required in the subdivided blocks</p>
	<p>Step 3: Blocks are further sub-divided; alleys, or private roads, and/or mid-block connections shall be introduced according to the zoning code</p>
	<p>Step 4: Lots shall be introduced based on the land use types and zonings' lot size requirements</p>
	<p>Step 5: As developments take place, building placement shall be according to the land use regulations.</p>

**FIGURE 3-1: BLOCK AND CIRCULATION**



- Block layout shall be determined by the blocks' location in relation to street types and spacing as shown in Table 3-2.

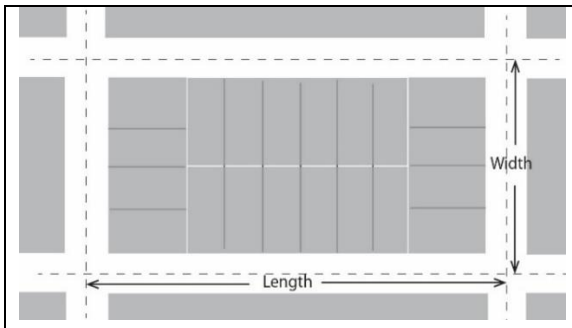
**TABLE 3-2: BLOCK DIMENSIONS AND STREET CONNECTIVITY STANDARDS**

Spacing Guidelines	Principal Arterials	Minor Arterials	Collectors	Neighborhood Collectors	Local Access Streets
Maximum Block Length (Public Street to Public Street)	660 feet	660 feet	660 feet	660 feet	660 feet
Minimum Block Size (Public Street to Public Street)	300 feet	250 feet	200 feet	150 feet	125 feet
Maximum Block Perimeter	2000 feet				
Maximum Distance between Bicycle and Pedestrian Facilities	330 feet	330 feet	330 feet	330 feet	330 feet

Note: In cases when block length exceeds 660 feet, or the block perimeter exceeds 2000 feet, mid-block pedestrian connections shall be provided. Exceptions are allowed when proposed buildings exceed the block length. In such cases, mid-block connections can be provided through the site or building as appropriate.

### 3.2.2.1 Residential blocks

- The width of the block shall be sufficient to allow for two tiers of lots with easements (Figure 3-2). Block Length shall be measured along the street centerline intersecting streets.

**FIGURE 3-2: RESIDENTIAL BLOCKS**

- Exceptions in the block length and width can be allowed for protection of natural resources, steep slopes, or when the lots front the shoreline (Figure 3-3).

**FIGURE 3-3: BLOCK LENGTH EXCEPTION**

### 3.2.2.2 Commercial and mixed-use blocks

1. Mixed-use projects are intended to be walkable and shall maintain block dimensions allowed in Table 3-1. For Mixed Residential and Commercial land use, block length can be allowed to increase from block dimensions allowed in Table 3-1 if the block includes mid-block connections (Figure 3-4), plaza, civic spaces, etc. Commercial land use shall provide mid-block connections (Figure 3-4) when block length exceeds 660 feet. In such cases, plaza, civic spaces, pedestrian walkways, alleys, or open space can be used to create mid-block connections. However, the block length must not exceed 1.5 times the length allowed in Table 3-2.
2. Exceptions in the block length may be allowed for protection of natural resources, steep slopes, or when the lots front the shoreline.
3. Mixed-use blocks are encouraged to use mid-block connections as shown in Figures 3-1 and 3-4.

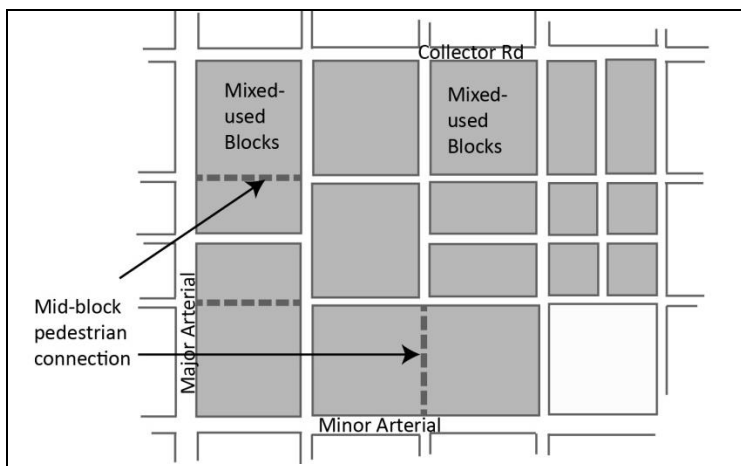


FIGURE 3-4: MID-BLOCK PEDESTRIAN CONNECTIONS

## 3.2.3 CIRCULATION AND CONNECTIVITY

### 3.2.3.1 Street classifications

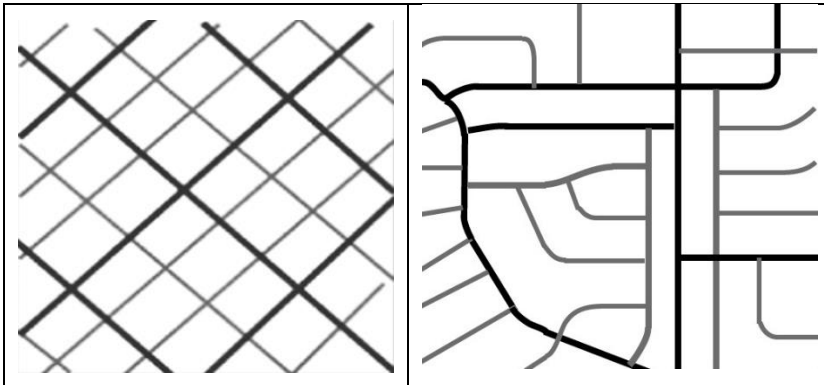
1. Streets shall be connected to develop a complete network. An interconnected circulation system provides linkages to local shopping, services, housing, and amenities, as well as linkages between adjacent developments. Street types and spacing shall be maintained according to Table 3-3. See additional standards and details in Section 3.6, Street Environment and Public Realm.
2. Alleys shall be allowed for service access, such as garages for residential uses, and loading services for commercial uses.

**TABLE 3-3: FACILITY SPACING STANDARDS**

Functional Classification	Maximum Spacing
Principal Arterial	1 to 2 miles
Minor Arterial	1 mile
Collector	½ mile
Neighborhood Collector	¼ mile
Local Access Street	660 feet
Bicycle and Pedestrian Facilities	330 feet

### 3.2.3.2 Street connectivity standards

1. Provide pedestrian and vehicular connections that link adjacent uses and circulation patterns, including buildings, public spaces, parking areas, roadways, sidewalks, and pathways.
2. An interconnected grid block and street network is required.
3. The street and block network is not required to be orthogonal and can change shape in response to site features and/or topographical constraints (Figure 3-5). However, streets shall be interconnected.



**FIGURE 3-5: INTERCONNECTED STREET LAYOUT IN GRID (ORTHOGONAL) AND CURVILINEAR PATTERN**

4. The street system of a proposed subdivision shall be designed to connect to existing, proposed, and planned streets adjacent to the subdivision. Wherever a proposed development abuts un-platted land or a future development phase of an existing development, street stubs shall be provided to allow access to future abutting subdivisions and to extend the street system into the surrounding area. Street ends shall contain turnarounds constructed to Uniform Fire Code standards and shall be designed to facilitate future extension in terms of grading, width, and temporary barricades.
5. Dedicating or deeding property for right-of-way or a portion thereof to the City for public streets within, or along the boundaries of all residential subdivisions or developments, shall

be required as a condition of application approval where the following can be demonstrated:

- a. Facts support that such dedication is reasonably necessary as a result of the impact created by the proposed development;
- b. Such dedication will result in proportionate mitigation of the impact in the reasonably foreseeable future;
- c. Connectivity to the existing or foreseeable future public right-of-way is feasible; and
- d. One or more of the following circumstances are met:
  - i. A city transportation plan indicates the necessity of a new or additional right-of-way or portion thereof for street purposes;
  - ii. The dedication is necessary to provide additions of right-of-way to existing right-of-way to meet city road standards;
  - iii. The dedication is necessary to extend or to complete the existing or future neighborhood street pattern;
  - iv. The dedication is necessary to comply with road standards and city transportation plans;
  - v. The dedication is necessary to provide a public transportation system that supports future development of abutting property consistent with the Comprehensive Plan or Pasco Municipal Code.
6. Cul-de-sacs/ dead-end streets are prohibited; except, where the Comprehensive Plan or preliminary plat indicates a street is to continue past the subdivider's property, the City may allow the dead end until such time as the street can be built through at a later date. Shared driveways and road-ends for shoreline public access are not considered cul-de-sacs/ dead-ends.
7. Shared driveway access from residential streets is allowed for attached dwelling units; for detached single-family residential lots, one shared access for four lots is allowed. For commercial developments, access must be shared with the adjacent development/parcels according to Table 3.4.

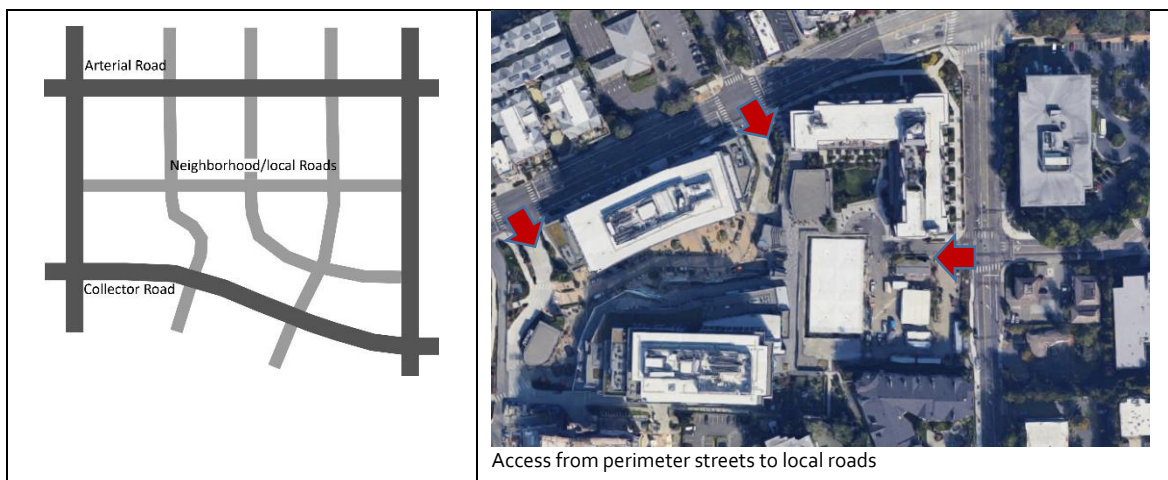
**TABLE 3-4: ACCESS MANAGEMENT STANDARDS**

Spacing Guidelines	Principal Arterials	Minor Arterials	Collectors	Neighborhood Collectors	Local Access Streets
Minimum Driveway Spacing (Driveway to Driveway)	300 feet	250 feet	150 feet	75 feet	N/A
Minimum Full-Access Driveway Spacing (Setback from Intersection)	300 feet	250 feet	150 feet	75 feet	25 feet
Minimum Right-In/Right-Out Driveway Spacing (Setback from Intersection)	150 feet	125 feet	75 feet	50 feet	25 feet
Note: Additional access and/or driveway management shall be determined at the development review phase					

8. Street standards shall comply with the standards mentioned in Section 3.6, Street Environment and Public Realm, and Pasco Transportation System Master Plan (TSMP).
9. Management of driveways and access will be in place on the following corridors: Broadmoor Boulevard, Sandifur Parkway, Burns Road Dent Road, and Road 108.

### 3.2.3.3 Additional standards for Mixed Residential and Commercial land use

1. Mixed Residential and Commercial land use shall have primary automobile access from arterial and/or collector streets with transit and bicycle circulation. Mixed-use developments shall provide pedestrian and bicycle access and circulation to surrounding residential areas.
2. Primary vehicular access to mixed-use developments shall be provided from perimeter arterial streets and/or perimeter collector streets to local access roads (Figure 3-6).

**FIGURE 3-6: CIRCULATION IN MIXED-USE DEVELOPMENTS**



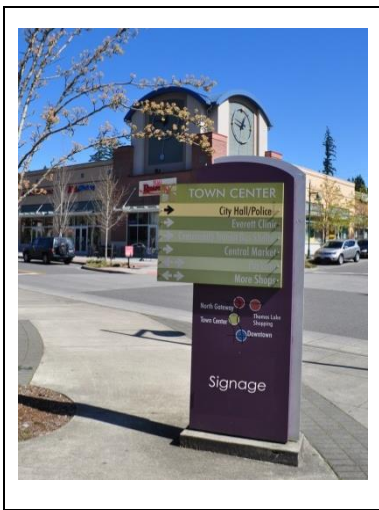
3. Internal circulation shall be provided through an internal street system and local access roads.
4. Private streets are allowed to provide internal site circulation within developments and according to the standards approved by the City.
5. The internal transportation network of the Mixed Residential and Commercial land use shall be designed to maximize multi-modal travel options. All transportation infrastructures shall meet the intent of the City's Complete Streets Ordinance, PMC 12.15, Best practices criteria, PMC 12.15.050 and comply with the International Fire Code. Dedication of public right-of-way is not required when private streets are used. Private streets may only be used with the approval of the City Engineer. Private streets may only be used for internal circulation streets.

#### 3.2.3.4 Transit

Developments shall coordinate with transit providers to plan for appropriate locations for bus stops and other transit facilities.

### 3.2.4 SIGNAGE AND LIGHTING

1. Developments shall consolidate business signage, one on each public street. This does not include wall-mounted signage in front of the businesses. Pole signs are discouraged. If used, pole signs shall be aesthetically pleasing with creative design, use of masonry, or similar material, and integrating landscaping.
2. Projects shall include pedestrian-scale wayfinding signs to be placed in clearly visible areas (Figure 3-7). Such signage can be placed on sidewalks, open spaces, and/or plazas when it does not block the sidewalk and vehicular visibility.



**FIGURE 3-7: PEDESTRIAN SCALE WAYFINDING SIGNAGE**

3. All light related to signage shall be in compliance with the PMC 12.32, Outdoor Lighting.
4. Signage in the community centers should be designed to reflect the centers' characteristics and uses.
5. Material used for signage shall be durable in nature and consistent with the design of the project.
6. The following signs shall be prohibited:
  - a. Signs with flashing/strobing light
  - b. Billboard signs
  - c. Signs, which by coloring, shape, wording, or location resemble or conflict with traffic control signs or devices
  - d. Signs that create a safety hazard
7. Parking lot and private street lighting shall meet industry and City illumination standards.

### 3.3 BUILDING TYPES AND STANDARDS

#### 3.3.1 INTENT

The intent of standards by building types is to acknowledge a variety of developments that will offer housing choices and create a vibrant community. These standards will provide greater flexibility and address the physical, functional, and aesthetic objectives of the master plan.

#### 3.3.2 RESIDENTIAL BUILDING TYPES

Residential building types allowed in various land uses and standards are described below. All residential site and building design standards, such as lot size, frontage, setback, parking, orientation, height, bulk etc. from the Pasco Municipal Code shall apply to residential developments in Broadmoor area.

##### 3.3.2.1 Single-family residential - detached

1. Traditional detached or single family detached dwelling means a free-standing dwelling unit designed for or occupied exclusively by one family that is not adjoined by any other dwelling units. (Figure 3-8).
2. Shared driveway: Two to four single-family homes with a shared driveway access from the street to the lot are allowed (Figure 3-8).
3. Cottage housing: Cottage housing is generally defined as a grouping of small, single-family dwelling units clustered around a common area and developed with a coherent plan for the

entire site (Figure 3-8). Units shall be built at or below 1,200 square feet. By design, cottage housing is geared toward single-family tenancy and can be constructed as either attached or detached units. Cottage housing developments should be focused around community courtyards where housing clusters numbering 4 to 12 units open onto the shared space.



**FIGURE 3-8: SINGLE-FAMILY DETACHED HOUSING**

### 3.3.2.2 Single-family residential - attached

1. **Townhomes:** Townhouses are defined as single-family dwellings with at least two floors and share at least one wall with another house (Figure 3-9). Unlike duplexes or multiplexes, however, each townhouse is individually owned on an individual lot. No dwelling unit is located above or below another dwelling unit. The building generally comprises of 4 units.
2. **Row houses:** A row house is a series or row of single-family attached dwellings units that faces the street (Figure 3-9). The building is generally composed of 5 or more attached 2- or 3-story dwelling units arranged side by side. The ground floor may be raised above grade to provide privacy for ground floor rooms. Each dwelling unit shares at least one common wall with another dwelling unit. No dwelling unit is located above or beneath another. The primary building sits at the front of the property, with the garage at the rear, sometimes separated from the primary building by a rear yard. Each row house is individually owned on an individual lot.

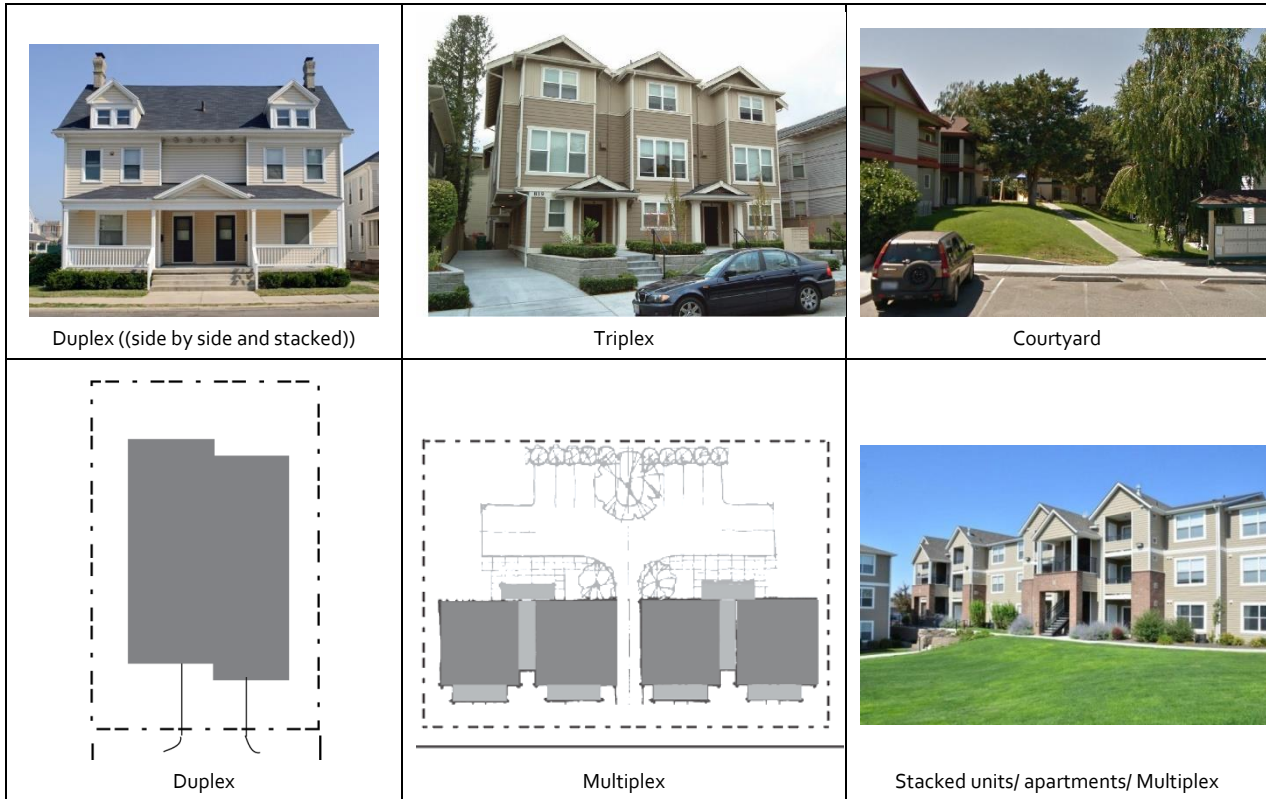


**FIGURE 3-9: SINGLE-FAMILY ATTACHED HOUSING**

### 3.3.2.3 Multi-family residential – medium to high density

3. Duplex style dwellings consist of a pair of dwelling units located side by side or one above the other to create one building on a single lot. Triplexes and fourplexes are 3 or 4 dwelling units located side by side or one above the other to create one building on a single lot (Figure 3-10). Also see the definition, dwelling unit, duplex in PMC.
4. Multiplex is a residential building consisting of four or more dwelling units. Depending on the lot size and context the units can be placed side by side, front-to-back or stacked, or some combination of these options (Figure 3-10).
5. Courtyard multifamily: Three or more dwelling units arranged around a shared courtyard, with pedestrian access to the building from the courtyard and/or fronting the street (Figure 3-10). Also see definition, dwelling unit, courtyard apartment in PMC.
6. Live-work unit: An integrated residence and work space located at street level, occupied and utilized by a single household in an array of at least 3 such structures, or a structure with at least 3 units arranged side by side along the primary frontage, that has been designed or structurally modified to accommodate joint residential occupancy and work activity (Figure 3-10).
7. Row house: Similar as defined in row house under single-family attached except that all units of the building share one lot (Figure 3-10).

8. Stacked unit apartments: This includes more than 1 dwelling unit in a single structure. It is not a cottage house, row house, or townhouse development (Figure 3-10). Stacked flats can be entirely residential, or in mixed-use building with residential units on top of ground floor commercial.



**FIGURE 3-10: MULTIPLE-FAMILY HOUSING TYPES**

### 3.3.3 MIXED, COMMERCIAL, AND CIVIC BUILDING TYPES

#### 3.3.3.1 Mixed use Buildings

1. Mixed-use developments integrate two or more land uses, such as residential with commercial and/or office that support each other with a strong pedestrian orientation. Mix of uses can be on a site in multiple buildings (horizontal), or in individual buildings (vertical), or a combination of both (Figure 3-11).
2. Horizontal mixed-use development incorporates residential and pedestrian-oriented commercial uses (and any additional uses allowed in the underlying zoning district) on the same lot or contiguous lots (Figure 3-11). This type of development may occur within a single building where uses are separated by walls or within multiple buildings.





**FIGURE 3-11: VARIOUS MIXED-USE BUILDINGS AND LAYOUTS**

3. Vertical mixed-use development incorporates residential and pedestrian-oriented commercial uses (and any uses allowed in the underlying zoning district) within the same building (Figure 3-11). This type of development may occur within a single building or multiple buildings on the same site or contiguous sites where uses are separated by floors.
4. Pedestrian-oriented commercial use is a commercial enterprise whose customers may likely arrive by foot and may include restaurants, retail shops, personal service businesses, travel services, banks (except drive-through windows), and other similar establishments that generally benefit from pedestrian activity (Figure 3-11).
5. Courtyard Mixed-Use Building. A mix of two building types, the stacked dwelling units, and courtyard housing arranged around a courtyard (Figure 3-11). This includes access to the

stacked dwelling and walk-up access to the courtyard housing. The building may include retail, service, or office uses in the ground floor, with upper floors for dwelling units.

6. **Linear Mixed-Use Building.** Linear building conceals a parking garage or large-scale buildings that do not need street frontage, such as movie theater, to create a pedestrian-friendly environment (Figure 3-11). The building can include retail, service, or office uses in the ground floor, with upper floors for retail, service or office uses, and dwelling units.

### 3.3.3.2 Commercial Buildings

1. **Commercial/Flex Building.** A Flex Building is designed for flexible occupancy by retail, service, and/or office uses on the ground floor, with upper floors configured for retail, service, office, and/or other similar uses (Figure 3-12). Second floor units may be directly accessed from the street through a stair; upper floors are accessed through a street level lobby.



**FIGURE 3-12: COMMERCIAL BUILDINGS**

### 3.3.3.3 Civic Buildings

1. **Civic buildings** are primarily intended for civic, community and institutional uses, and generally define the neighborhood. Buildings include schools, public safety buildings, libraries, recreation, and other community buildings (Figure 3-13). Some facilities carry

significance for the community, and building forms are reflective of such intent, such as locating them at a street terminus, or visible with ample setback, taller structure, etc.



**FIGURE 3-13: CIVIC BUILDINGS**

### 3.4 DEVELOPMENT STANDARDS BY LAND USE

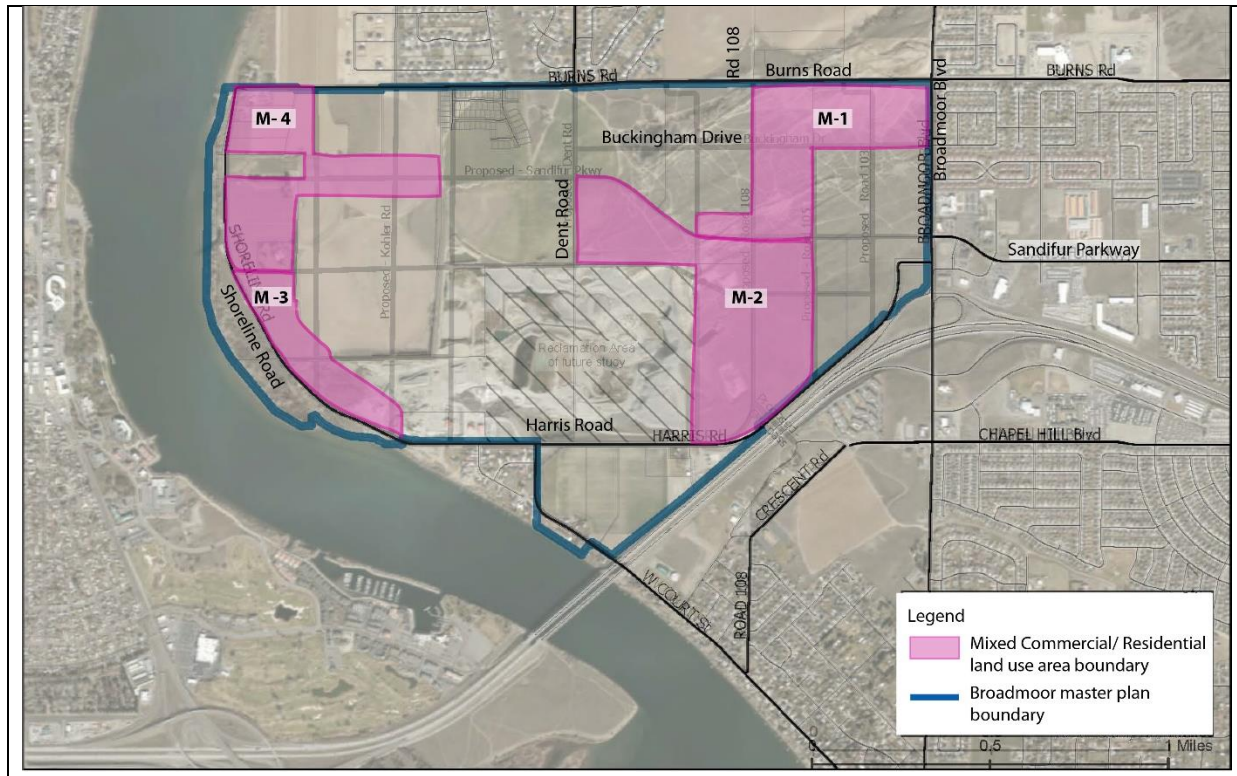
The following standards shall apply to each land use and its subsequent zoning. Low density residential land uses under R-1 zone shall not be allowed except for subdivisions that have been already approved under Low Density Residential land use.

#### 3.4.1 PERMITTED LAND USES

##### 3.4.1.1 Allowed and prohibited uses

1. This section establishes the land uses allowed in each land use designation and the corresponding permit requirements. A parcel or building subject to the Broadmoor Master Plan shall be occupied with only the land uses allowed by Tables 3-5 through 3-8. Definitions of allowed uses are provided in Section 3.3, Building Types and Standards, Section 6, Definitions and in PMC Chapter 25.15, Definitions. Table 3-9 indicates uses that are prohibited in the Broadmoor area. If a land use is not identified in this section, the Planning Director may determine that the use is not permitted, or determine the appropriate definition and the proposed use to be permitted, provided the Planning Director makes the findings in writing that the use will be compatible and consistent with the purpose of the Broadmoor Master Plan.
2. Uses within Reclamation land use are not identified in this development regulations. Uses in this land use category shall be determined by the City upon completion of a reclamation study by the Department of Natural Resources.
3. Uses within Industrial land use are not identified in this development regulations. Uses in this land use category is intended to allow the continuation of existing industrial uses under the current industrial regulation in the Pasco Municipal Code until 2035. After that, the industrial land use will be amended to Medium Density Residential.
4. Mixed Residential and Commercial land uses shall be allocated in each Mixed Residential and Commercial land use planning area (Figure 3-14). Mix of land use shall contain a minimum of 20% residential and 20% of non-residential use in each planning area.
5. Mixed Residential and Commercial land use can coordinate with another land use that is located within close proximity (1/4 mile) in order to meet the intent of the horizontal mixed use. For example, when Mixed Residential and Commercial land use is located next to Medium Density Residential land use, Mixed Residential and Commercial land use area can have a commercial development next to the Medium Density Residential land use area in accordance with the mixed-use standards for commercial uses. Each single use (residential and non-residential) in this case can be considered a part of a combined mixed-use development.





**FIGURE 3-14: MIXED RESIDENTIAL AND COMMERCIAL LAND USE AREA**

**TABLE 3-5: RESIDENTIAL USES**

<p>"P" – permitted subject to applicable standards in this code. Allowed uses will be permitted in structures that have already gone through a site and building review process.</p> <p>"C" – permitted subject to the conditional use provisions</p> <p>"X" – not permitted</p> <p>Uses not listed in this table are not allowed.</p>					
Land Use	LDR	MDR	Mixed R-C	C	OS
Single-family homes - detached	P	P	X	X	X
Single-family homes - attached	P	P	P	X	X
<ul style="list-style-type: none"> <li>Townhomes/ row houses</li> <li>Duplexes, triplex, fourplex</li> <li>Cottage House, Courtyard Housing (4-12 units)</li> </ul>					
Live/work unit	X	P	P	X	X
Apartments/Multiple dwellings/Condominiums	X	P	P	C	X
Manufactured homes/ Factory-assembled homes	P	P	C	X	X
Nursing homes and assisted living facilities	C	C	P	X	X
Adult family home (6 or fewer)	P	P	P	X	X
Family home daycare facility	P	P	P	X	X
Bed and breakfast	P	P	P	X	X



**TABLE 3-6: COMMERCIAL AND OFFICE USES**

<p>"P" – permitted subject to applicable standards in this code. Allowed uses will be permitted in structures that have already gone through a site and building review process.</p> <p>"C" – permitted subject to the conditional use provisions</p> <p>"X" – not permitted</p> <p>Uses not listed in this table are not allowed.</p>					
Land Use	LDR	MDR	Mixed R-C	C	OS
Administrative and professional offices, banks, dancing schools	X	X	P	P	X
Medical and dental offices	X	X	P	P	X
Funeral homes	X	X	X	P	X
Automobile detail shops and automobile rental or leasing	X	X	X	P	X
Hotels and motels	X	X	P	P	X
Restaurants/ café/ breweries and similar uses	X	X	P	P	X
Retails stores and shops, e.g., bakeries, groceries, beauty shops or similar uses	X	X C <sup>1</sup>	P	P	X
Filling stations (gasoline and/or electric)	X	X	P	P	X
Car dealership (showroom sale only)	X	X	P	P	X
Car wash, repair	X	X	P	P	X
Entertainment and recreational uses (e.g., theaters)	X	X	P	P	X
Veterinarian clinics for household pets (no boarding or outdoor treatment facilities)	X	X	P	P	X
Wholesale	X	X	X	P	X
<p>1. Allowed for live work units and for neighborhood commercial uses with buildings less than 10,000 sq-ft in size; does not include auto-related uses and drive-throughs businesses.</p>					

**TABLE 3-7: CIVIC USES**

<p>"P" – permitted subject to applicable standards in this code. Allowed uses will be permitted in structures that have already gone through a site and building review process.</p> <p>"C" – permitted subject to the conditional use provisions</p> <p>"X" – not permitted</p> <p>Uses not listed in this table are not allowed.</p>					
Land Use	LDR	MDR	Mixed R-C	C	OS
Museums and galleries	C	C	P	P	C
Governmental/ civic offices	C	C	P	P	C
Police and fire stations	C	C	P	P	C
Libraries	P	P	P	P	C

"P" – permitted subject to applicable standards in this code. Allowed uses will be permitted in structures that have already gone through a site and building review process.

"C" – permitted subject to the conditional use provisions

"X" – not permitted

Uses not listed in this table are not allowed.

Land Use	LDR	MDR	Mixed R-C	C	OS
Community event centers, civic facilities, recreational centers	P	P	P	P	P
Hospitals, clinics	C	C	P	P	X
Churches and other places of worship	C	C	C	P	X
Educational institutions	C	C	P	P	C
Daycare centers and preschool centers	C	C	P	P	X
Parks, public and private open spaces	P	P	P	P	P

**TABLE 3-8: OTHER USES**

"P" – permitted subject to applicable standards in this code. Allowed uses will be permitted in structures that have already gone through a site and building review process.

"C" – permitted subject to the conditional use provisions

"X" – not permitted

Uses not listed in this table are not allowed.

Land Use	LDR	MDR	Mixed R-C	C	OS
Cottage industries (repair, crafts, Hi-tech etc.)	X	X	C	C	X
Business incubators	X	X	P	P	X
Printing shops	X	X	P	P	X
Existing mineral extractions	X	X	X	X	X

**TABLE 3-9: PROHIBITED USES**

"X" – not permitted	
Land Uses	All Land Uses
<ol style="list-style-type: none"> <li>1. Automobile sales / Car dealership</li> <li>2. Automobile assembly services,</li> <li>3. Repair auto body shops<sup>1</sup></li> <li>4. Mini-storage facilities</li> <li>5. Pawn shops</li> <li>6. Adult bookstores or entertainment facilities</li> <li>7. Truck stops – diesel fuel sales</li> <li>8. Truck terminals</li> <li>9. Heavy machinery sales and service</li> <li>10. Contractor's plant or storage yards</li> <li>11. Mobile home and trailer sales and service</li> <li>12. Veterinarian clinics for livestock, including outdoor treatment facilities</li> </ol>	X

"X" – not permitted	
Land Uses	All Land Uses
13. Any outdoor manufacturing, testing, processing, or similar activity 14. On-site hazardous substance processing and handling or hazardous waste treatment and storage facilities 15. Junkyards, automobile wrecking yards, scrap iron, scrap paper, or rag storage, sorting, or baling 16. Recreational vehicle parks 17. Outdoor storage of RVs, boats, and trailers (not individual lot specific) <sup>2</sup>	
Notes: 1. Allowed in Commercial land when such uses are adequately screened with landscaping buffer from abutting residential and mixed-use neighborhoods, and the site and building design is compatible with the surrounded uses 2. Prohibited in Commercial land, allowed as part of residential neighborhoods when screened in such a way that they are not visible from public streets, abutting residential and mixed-use neighborhoods. Such screening must be compatible with the design of the surrounding neighborhoods.	

### 3.4.2 MEDIUM DENSITY RESIDENTIAL

This land use will implement the standards in this section and standards from PMC R-2, R-3, R-4, and PUD zoning. Standards in this section will take precedence over R-2, R-3, R-4, and PUD zoning standards.

#### 3.4.2.1 Intent

The intent of this land use is to allow small lot single-family homes, both attached and detached, as well as duplexes, triplexes or multiplexes, and townhomes in this land use.

#### 3.4.2.2 Density

Density in this land use is 6 to 29 dwelling units per acre. Density will vary in different zoning districts.

#### 3.4.2.3 Lot standards

**TABLE 3-10: LOT SIZE IN ZONING**

Building Type	R-2	R-3	R-4
Single-family detached (minimum)	5,000 sf/du	3000 sf/du	2000 sf/du
Single-family residential - attached			
Townhome/Row House (minimum)	3,000 sf/du	2,000 sf/du	1,500 sf/du
Duplex, Triplex, Fourplex (minimum)	2,500 sf/ du	2,000 sf/du	1,500 sf/du
Cottage / courtyard housing (minimum lot size for 4 units minimum to 12 units maximum) <sup>2</sup>	2,000 sf/du	1,800 sf/du	1,500 sf/du
Multiplex (minimum)	Based on the Comprehensive Plan density, site design and parking standards		
Notes: Lot size averaging shall apply according to PMC 21.20.070, Lot Size Averaging.			

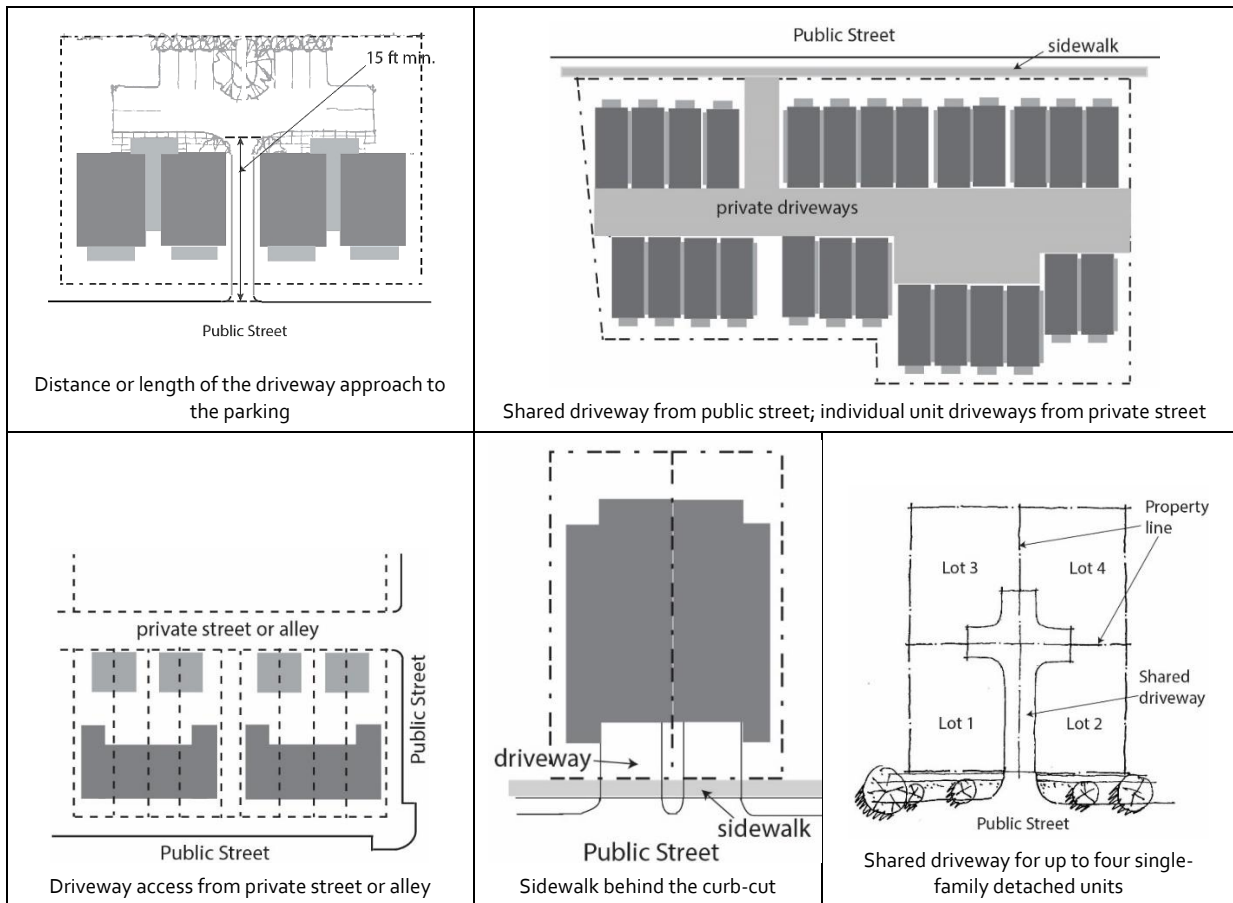
1. Lot standards shall comply with the PMC 25.22 standards for R-2, R-3 and R-4 zoning districts. A maximum 20% of deviation from lot size, width, and frontage can be allowed if the average of all lots in the development complies with the standards, and if the development achieves a better quality of development by deviating from these standards. Lot size averaging must not reduce the net density. Tables 3-10 indicate lot size requirements.
2. Lot width, frontage, coverage, and building height and setback shall be according to PMC 25.22 standards for R-2, R-3 and R-4 zoning districts.

#### 3.4.2.4 Access and driveways

1. For multiple units, the distance or length of the driveway approach to the parking shall be no less than 15 ft (Figure 3-15).



2. Vehicular access and driveway locations shall be located to minimize impact on pedestrian/bike routes.
3. Shared driveways are allowed for maximum 4 detached dwelling units, and for all attached dwelling units (Figure 3-15).
4. Duplexes, triplexes and fourplexes shall have shared driveway access from public street.
5. Minimum width of shared driveway, and/or internal access roads shall be 26 ft with a turning radius of 26 ft except for PMC 21.20.060.



**FIGURE 3-15: DRIVEWAY OPTIONS**

6. When driveways of individual dwelling units consist of more than 50% of the lot width, developments must avoid repeated curb-cuts from public streets in order to provide safe pedestrian movement. At least one of the following design approaches shall be followed (Figure 3-15):
  - a. Use shared driveways in order to reduce individual driveways from the main access road.
  - b. Driveway access from private streets and/or alleys.

- c. Place the sidewalk behind the curb-cut and with minimum 20-foot driveway depth from the back of the sidewalk.
- 7. For multi-units, internal access roads connecting parking lots and dwelling units shall be easily identifiable.
- 8. For apartments and multiplexes, dedicated pedestrian network between parking lots, public streets and sidewalks, open spaces, and community facilities shall be provided.
- 9. Developments shall provide pedestrian and bicycle access within and onto the site. Access points onto the site shall be provided (a) approximately every 330' to 660' feet along existing and proposed perimeter sidewalks and walkways, and (b) at all arrival points to the site, including abutting street intersections, crosswalks, and transit and school bus stops. In addition, access points to and from adjacent lots shall be coordinated to provide circulation patterns between sites.
- 10. Walkways shall form an on-site circulation system that minimizes the conflict between pedestrians and traffic at all points of pedestrian access to on-site parking and building entrances. Walkways shall be provided when the pedestrian access point onto the site, or any parking space, is more than 60' feet from the building entrance or principal on-site destination.
- 11. A cluster or multiple clusters of dwelling units and parking shall be considered to break down the long and linear circulation pattern.
- 12. A network of pedestrian connections shall be provided between parking lots, street sidewalks, open spaces, community facilities and buildings.
- 13. Pedestrian walkways shall connect with public parks, open spaces and/or common areas within the development and in the vicinity.
- 14. Pedestrian access must be clearly visible from the street and parking lots.
- 15. Minimum width of a hard surfaced walkway shall be according to the ADA standards.
- 16. Walkways must be adequately lit to meet the industry and City standards. All lighting, material and surface standards shall meet the City code.
- 17. Walkways must be maintained by the owner or owner's association.

#### 3.4.2.5 Setbacks

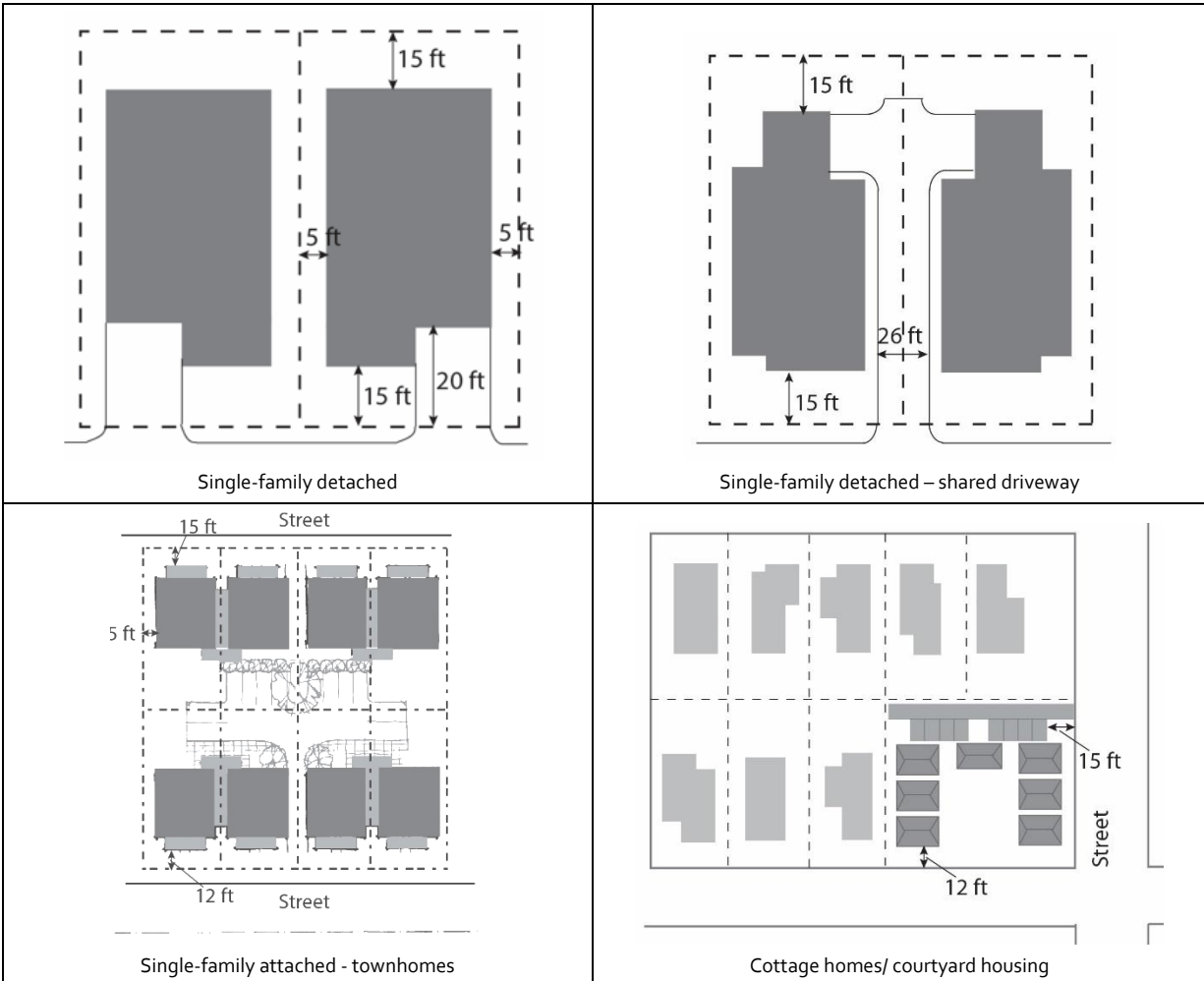
- 1. Building setbacks shall be according to Table 3-11 and the standards in this Section 3.4.2.5. Also see Figure 3-16.

TABLE 3-11: BUILDING SETBACKS

HOME TYPE	Minimum Building Setbacks		
	Front	Rear	Side
SFDU – Detached, Duplexes (Living Unit)	15’*	15’	5’
SFDU – Attached, Townhome, Cottage Housing, Multiplex/Apartments^	12’**,+	15’	5’
*Permitted when/if attached garage is setback at least 20’, or accessed via rear alley or shared private driveway; no encroachment of a utility easement permitted			
**Permitted with shared driveway is used and parking is located in middle or rear of development			
+ When building is accessed directly from public streets, standards of SFDUs-Detached shall apply			
^Multiplex/Apartments or mixed-use buildings permitted without a minimum setback if abutting a sidewalk is 8’ or wider and has garage or parking located on the back or side of the site. A 5’ landscaping buffer setback is required when the abutting sidewalk width is less than 8’			

2. Front yard setback for single family detached dwelling units and duplexes shall be as follows:
  - a. The living unit of a home may be set back from the back of the sidewalk or property line whichever is closer to the building a minimum of 15 feet if the attached garage is setback at least 20 feet from the back of the sidewalk, or if the garages are accessed from alleyways or private shared driveway.
  - b. A setback of 15 ft from the back of the sidewalk is allowed when parking and garages are located on the rear side
3. Front yard setback for single family attached dwelling units and multiplexes shall be as follows:
  - a. A setback 12 ft from the back of the sidewalk or property line whichever is closer to the building is allowed when shared driveway is used, and parking area is located in the middle, side of rear side of the development. Building structure shall not encroach upon any utility easements required by the City.
  - b. When buildings are accessed directly from a public street without a shared driveway, standards #2 of this Section 3.4.2.5 shall apply.
  - c. Multiplex/apartments buildings, or townhomes as part of a mixed-use building are allowed without any setback when they abut an 8 ft or wider sidewalk (including street tree planting boxes), and the building has garage parking or parking is located on the side or back of the building.

- i. For sidewalks less than 8 ft wide, multiplex buildings shall add a minimum of 5 ft of landscaping in between the sidewalk and building, or expand the sidewalk for an additional 5 ft to be maintained by the property owner, or the home owners' association. Additional setback may be required to accommodate easements, street or sidewalk improvements, or public plazas.



**FIGURE 3-16: SETBACKS AND DRIVEWAY STANDARDS BY BUILDING TYPES**

4. Side yard setback shall be 5 ft for all housing types
5. No side yard setback is required for attached dwelling units in zero lot lines; corners of zero lot lines shall have 5 ft setback
6. Rear yard setback shall be 15 feet for all housing types.
7. Rear yard setback for single family attached dwelling units and multiplexes shall be 15 feet minimum.



8. Lots with alleyways or private driveways and garages accessed from alley shall have 5 ft rear yard setback for the garage portion from alley. Additional setback may be required if needed for vehicle turning radius into garage. Rear yard setback for the living portion shall be according to standards identified in this section.

#### 3.4.2.6 Additional design considerations

1. All residential site and building design standards in the PMC 25.175, such as treatment of the corner lots, entrance, orientation, open space, landscaping, signage, lighting, building height, bulk and scale shall apply.
2. Residential developments should orient to surrounding uses, including residential and non-residential areas, in a way that encourages compatible development patterns, character, and appearances.
3. Where appropriate, incorporate and enhance significant natural features as site amenities and/or design elements.
4. Address the critical issues of site layout that influence a compatible and integrated neighborhood character, including, but not limited to, vehicular access, pedestrian connectivity, building orientations, and common spaces.
5. Limit street connections for individual residential homes off major roadways, including highways, principal arterials, and other designated mobility corridors, to mitigate development impacts on the roadway system.
6. Site entryways, primary circulation patterns, and connections to adjacent uses should appear and function like complete streets with sidewalks and pedestrian amenities rather than oriented to accommodate only vehicles.
7. Strategically locate common and open spaces and site amenities to encourage and support pedestrian activities.

### 3.4.3 MIXED RESIDENTIAL AND COMMERCIAL

#### 3.4.3.1 Intent

The purpose of Mixed Residential and Commercial land use is to encourage mixed-use developments for efficient use of land and public services, increase economic return, reduce transportation costs, encourage human interaction, create place-making with day and evening activities, create a more sustainable development pattern, and increase development opportunities.

The notion of mixed use for the Broadmoor area is to accommodate uses that are compatible, supportive, and complement each other. Various land uses in the Mixed Residential and Commercial land use designation should be placed in close proximity. To that effect, mixed use can be horizontal in a block setting, or vertical in a building setting as shown in Figure 3-17.



**FIGURE 3-17: HORIZONTAL AND VERTICAL MIX OF USES**

#### 3.4.3.2 Density and mix of uses

1. At least 20% of the combined gross floor area of the buildings proposed within each of the four Mixed Residential and Commercial land use planning areas (M-1, M-2, M-3 and M-4) shown in Figure 3-14 shall be devoted to residential uses, and at least 20% to non-residential uses. The 80%/20% thresholds for residential, commercial, and non-residential uses shall be applied based on the following criteria:
  - a. Use within a specific mixed-use area (M1, M2, M3 or M4)
  - b. Use is located within and/or overlapping a mixed-use area and/or different land use, in conformance with 3.4.3.2(1)
  - c. Uses are within 1/4-mile proximity
2. Parking garages or portions of garages devoted to residential or non-residential uses may be counted toward this requirement, but surface parking may not.

3. Maximum of one single use can be 80% of the total development and/or the floor area, whichever applies.
4. The Planning Director may approve ratios and/or density lower than the minimum up to 10% if the project meets the overall intent of the Mixed Residential and Commercial land use.
5. Public uses shall be based on design general standards.

#### **Residential Mix**

1. For vertical mixed-use projects, minimum 20% of the building area for vertical mixed use, or 20% of the total building project wide shall be residential.
2. Density of minimum 21 dwelling units per acre for the residential portion of the site shall be required.

#### **Commercial/ Non-Residential Mix**

1. Minimum 20% of the building area for vertical mixed use, or 20% of the total building/floor area on the site shall be commercial or a non-residential use.

#### **3.4.3.3 Lot standards**

##### **Size**

1. Lot standards are flexible, and shall be according to the mixed-use block standards in sub-section 3.2.2.2.
2. Residential portions of developments for horizontal mixed-use shall be according to lot standards as mentioned in Medium Density Residential Section 3.4.2.3, Table 3-10, Lot Size in Zoning, and PMC 25.22 standards for R-2, R-3 and R-4 zoning districts.

#### **3.4.3.4 Building design standards**

##### **Height**

1. Mixed use with ground floor retail shall have a maximum height of 90 feet including upper-level step backs.
2. Residential building shall maintain maximum building height according to PMC 25.22 unless mentioned in this Section.
3. Maximum building height for multiplex/multifamily adjacent to commercial areas in a horizontal mixed-use shall be 90 feet including upper-level step backs.

### Active Ground Level and Building Design

1. For vertical mixed use, and commercial portion of horizontal mixed use, ground levels shall be designed to promote easy pedestrian access. Human scale design shall include:
  - a. Linkages between sidewalks and building access
  - b. Store frontage with at least 50% of transparent façade on the street side and/or pedestrian access side
  - c. Building form and scales that relate to pedestrians, such as wider sidewalk at the ground level, awning, canopies, pergolas, etc.
2. For vertical mixed use, and commercial portion of horizontal mixed use, buildings should be designed with architectural features such as:
  - a. Projecting or recesses elements, varying roofline, upper level step back (Figure 3-18)
  - b. Modulation and articulation. Vertical articulation of buildings shall be at least at every 50 feet.



**FIGURE 3-18: GROUND FLOOR RETAIL AND UPPER LEVEL STEP BACK**

3. At least 30% of the floor area of the ground floor or a portion of a mixed-use development must be dedicated to commercial uses that are accessible to pedestrians. when located on streets other than principal arterials. These standards shall be applicable for street frontages on arterial streets when they are the main street frontages.
  - a. Parking garages can be included in this count as long when they do not consist of more than 25% of the site or street frontage. These standards shall be applicable for



street frontages including arterial and collector street frontages when they are the main street frontages.

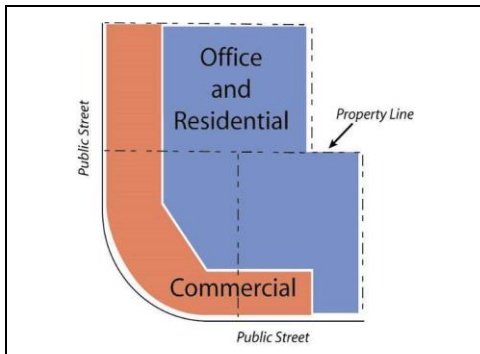
4. Commercial building frontages shall have at least 50% of transparent façade on the street side. Blank facades shall not face public streets. In case when such facades are unavoidable, blank facades shall be treated with trellises, modulation of the façade, display windows, artwork, lighting, and enhanced landscaping treatment.
5. Service areas such as utilities, dumpsters, loading docks, etc. shall be located in the less visible side of the site to avoid negative visual impacts on the street environments. They shall not face a major public street. In cases where there are no other options, backside facing public streets must be screened from public view with landscaping, and/or fencing that are consistent with the overall building design.
6. Roof-mounted mechanical equipment must be located and screened so the equipment is not visible from the ground level of adjacent streets or properties. Color of roof-mounted equipment should be matched with the exposed color of the roof to minimize visual impacts whenever possible.
7. Other standards such as Section 3.6, Street Environment and Public Realm shall apply to activate buildings' ground level with the street environment.

#### 3.4.3.5 Access and driveways

1. Standard driveway width for individual dwelling units (attached) for residential uses: 15 feet minimum; 20 feet maximum
2. Shared driveway width for all attached dwelling units shall be 26 feet minimum, 30 feet maximum. This doesn't apply to multiplexes and/or vertical mixed-use buildings.
3. Additional access and driveway standards from Section 3.4.2.4, Access and driveways shall apply for residential uses in a horizontal mixed-use setting.
4. For vehicular and pedestrian accesses, see Section 3.2.3, Circulation and Connectivity.
5. Each mixed-use building must have clearly visible front entrance accessible by pedestrians from the street. Residential entries should be designed to provide separation between front doors and the street environment.
6. Street crossings/crosswalks for pedestrian shall be designed to enhance visibility and safety; they shall use clear demarcation, painted pavement surface, raised sidewalk at the crossing, or similar treatment.

### 3.4.3.6 Compatibility

1. Compatibility of Uses. Land uses in mixed-use sites shall be selected and designed to encourage interaction among uses (Figure 3-19). Examples include but are not limited to commercial developments that are significantly used for shopping by on-site or adjacent residents or office workers.
2. All buildings and improvements on the site shall be located and designed to look and function as an integrated development and to encourage pedestrian travel between buildings and uses.



**FIGURE 3-19: INTEGRATED MIXED-USE DEVELOPMENT**

3. Multiple uses can be designed to be compatible with each other visually and functionally through massing, building material, façade treatment, and orientation (Figure 3-20).



**FIGURE 3-20: COMPATIBLE BUILDING DESIGN**

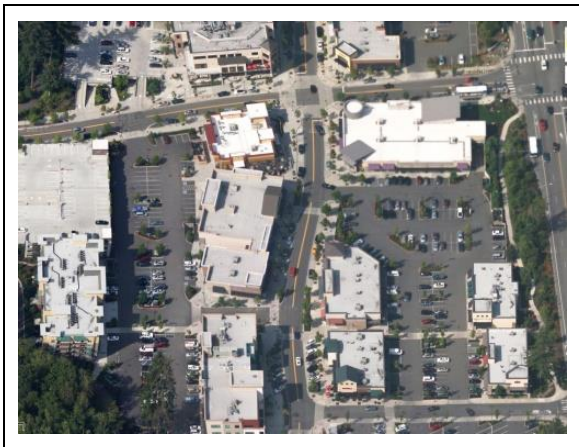
### 3.4.3.7 Setbacks

1. Residential buildings in horizontal mixed-use developments shall comply with the setback standards in Section 3.4.2.5, Setbacks and 3.4.3.8 Building Placement and relationship with Street, whichever is applicable.
2. Commercial and vertical mixed-use buildings:

- a. No setback is required for buildings. However, buildings shall not encroach on utility easements.
- b. Setbacks shall be consistent to make a pedestrian-friendly street frontage except when buildings setbacks are used to create plazas, open space and public amenities.
- c. Majority of setback areas shall be dedicated to public realm for pedestrian activities such as wider sidewalk, plazas, public open spaces, outdoor seating areas, etc. See Section 3.4.3.8, Building Placement and Relationship with Streets, and Section 3.6, Street Environment and Public Realm.

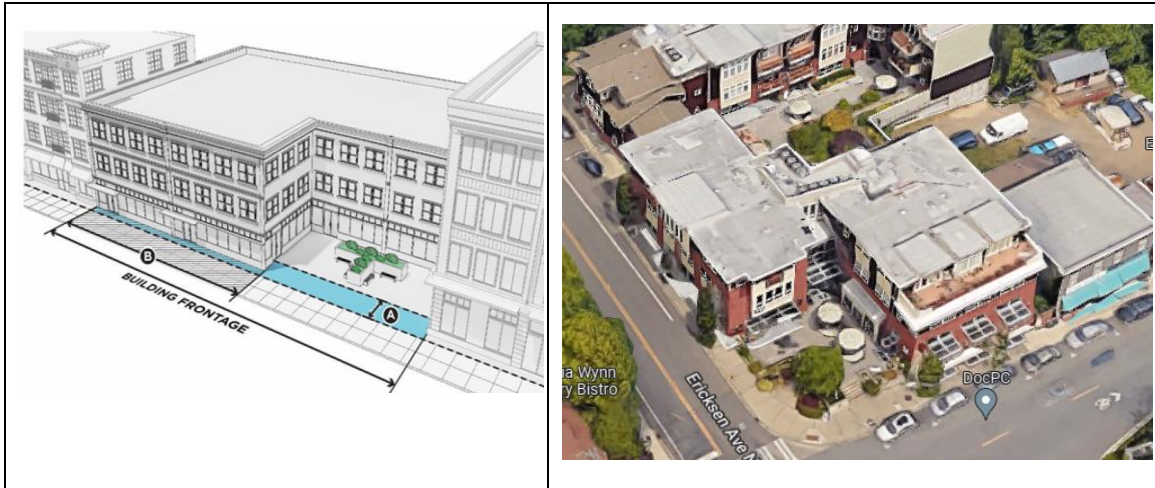
#### 3.4.3.8 Building placement and relationship with streets

1. For horizontal mixed use project site applications, commercial portion of the development should be primarily on the public street frontage while residential uses can be located further away from the primary street.
2. Parking shall be located behind the buildings (Figure 3-21) and shall not be allowed between buildings and the street within community centers and corridors (Figure 3-26) except as allowed in section 3.7.



**FIGURE 3-21: BUILDINGS CLOSE TO THE STREET**

3. Vertical mixed-use buildings of non-residential buildings that are part of mixed-use projects must be located close to the street. When abutting a corridor street, such buildings shall be within 10 feet of the inward from the property line, and for all other streets within Mixed-Residential and Commercial land use, buildings shall be within 20 feet of the inward from the property line, except for when buildings are set back to create plazas, open space and public amenities (Figure 3-22). This does not prevent additional buildings to be located away from the street in a multiple building setting.



**FIGURE 3-22: BUILDING PLACEMENT WITH PLAZAS AND COURTYARDS**

4. Corners of community centers and corridor intersections shall be treated with plazas, wider sidewalks, landscaping, artwork, and street furniture such as seating, benches, bike racks, etc. (Figure 3-23).



**FIGURE 3-23: TREATMENT OF STREET CORNER WITH PLAZAS AND OUTDOOR SEATING**

5. The primary access to buildings shall be visually prominent and easily accessible by pedestrians. Street frontages and intersections shall be enhanced with sidewalk, front court, plazas.
6. Separate buildings shall be connected through pedestrian linkages delineated through landscaping, differentiated surface materials, or texture.
7. Blank façade shall be treated with trellises, modulation of the façade, display windows, artwork, lighting, etc.

### 3.4.4 COMMERCIAL

#### 3.4.4.1 Intent

The intent of this land use is to allow various commercial uses to serve the area such as retail, wholesale, service, and ancillary office uses, as well as appropriate public uses such as government that meet the vision for the Broadmoor area to become a vibrant pedestrian-friendly community with quality design.

#### 3.4.4.2 Development standards

1. Development standards such as density, intensity, setback, height, parking, etc. shall be according to the City's zoning codes for commercial uses. Additional standards are described below that will take precedence in the case of a conflict with the City's zoning codes for commercial uses.

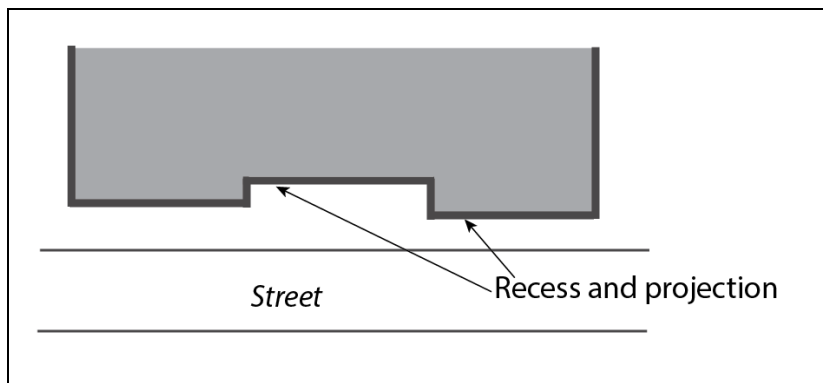
#### 3.4.4.3 Building placement and relationship with streets

1. Buildings shall be located close to the street to enhance streetscape character.
  - a. Parking shall not be allowed between buildings and the street.
  - b. In cases when parking abuts the street, standards from Section 3.7.2(5) shall apply.
2. Commercial buildings abutting Mixed Residential and Commercial land use, and/or located within the community centers and corridors (Figure 3-26) shall be within 10 feet of the inward from the property line except for when buildings are set back to create plazas, open space and public amenities (Figure 3-22).
3. Parking shall not be allowed between buildings and the street within community centers and corridors (Figure 3-26). Corners of such community centers and corridors shall be treated with plazas, wider sidewalks, landscaping, artwork, and street furniture such as seating, benches, bike racks, etc. (Figure 3-23).
4. Both pedestrian and vehicular access to the site shall be clearly visible and safe.
5. Buildings should be oriented to face public streets, open space, or plazas.
6. Blank facades should not face public streets. In case when such facades are unavoidable, blank facades shall be treated according to the standards in Section 3.4.4.4, Building Design.
7. Buildings are encouraged to be oriented to reduce exposure to strongest summer sunlight and glare from reflecting surfaces.
8. Service areas shall be located away from public streets.



#### 3.4.4.4 Building Design

1. Building designs should appropriately address building scale, mass, and form, and the use of materials, architectural elements, and details to ensure a compatible and aesthetic built environment.
2. Entrance to the building must be prominent with entry doors, porches, protruded or recessed entrances and shall be easily accessible from the adjacent public street and parking lots.
3. Primary pedestrian entrances must face public streets, open spaces, or plazas, whichever is available.
4. The street facing facade shall be designed to break down the bulk and mass into pedestrian scale by using the at least two of the modulation and articulation techniques as follows:
  - a. Change in the roof or wall plane. For wall plane (façade modulation), use recesses and/or projections (Figure 3-24)
  - b. Projected or recessed elements, canopies, cornices, etc.
  - c. Upper-level step back when feasible



**FIGURE 3-24: FAÇADE MODULATION**

5. Blank façades shall be treated with trellises, modulation of the façade, display windows, artwork, lighting, etc.
6. For commercial buildings, façade facing public streets or public open spaces or plaza shall have at least 25% of the façade area composed of clear vision glass unless a specific alternative design provides a better façade treatment and is approved by the Planning Director.
7. For ground floor retail, at least 40% of the ground floor façade shall be oriented towards, or face public streets or public open spaces or plazas, unless a specific

- alternative design provides a better façade treatment and is approved by the Planning Director.
8. Service areas such as utilities, dumpsters, loading docks, etc. shall be located in the less visible side of the site to avoid negative visual impacts on the street environment. They shall not face a major public street. In cases where there are no other options, backside facing public streets must be screened from public view with landscaping and/or fencing consistent with the overall building design.
  9. Roof-mounted mechanical equipment must be located and screened so the equipment is not visible from the ground level of adjacent streets or properties. Color of roof-mounted equipment should be matched with the exposed color of the roof to minimize visual impacts whenever possible.

### 3.4.5 OPEN SPACE

#### 3.4.5.1 Intent

Open space land use provides various functions, such as active and passive recreation, public realm, pedestrian interaction, contrasts to the built environment, environmental protection, etc. Open Space land use is intended to:





- Provide various recreational opportunities in parks and recreational areas
- Protect public and/or private natural areas
- Offer scenic qualities in a built environment
- Protect sensitive or fragile environmental areas and functions of the environment
- Preserve the capacity and water quality of the stormwater drainage system
- Provide pedestrian and bicycle transportation connections.

Open space requirements are applicable to all developments within the Broadmoor Master Plan area. Open spaces are intended to be included within other land uses even if the land use designation is not Open Space. This includes plazas, trails, courtyards etc. (Table 3-12).

#### 3.4.5.2 Standards

1. Standards for various types of open spaces shall be in compliance with zoning, streetscape, critical areas, and shoreline regulations and parks and recreation standards as applicable. However, certain open spaces identified in this section may retain the underlying zoning and land use designation of the primary use, such as private plazas, courtyards etc.
2. Open spaces shall be connected with bike and trail facilities. Some open spaces such as plazas can be extensions of the street environment as well as connected with the trail system.
3. Developments shall provide open space in the form of parks, plazas, courtyards, bike and trail connections and natural and landscaped open space. Lands for public parks and open space shall be required in accordance with the City's Parks, Recreation and Open Space Plan's LOS requirements.
4. Additional design considerations for Open Space are indicated in Table 3-12.

TABLE 3-12: OPEN SPACE STANDARDS

	Natural Open Spaces	Parks	Plazas	Trails/Greenways
				
Open space types	An open space intended for preservation in its natural condition; this also includes environmentally critical areas; unstructured and limited number of structures can be allowed.	Parks are a combination of active and passive recreational uses, such as sports (active), fishing (passive) as indicated in the City's PROS Plan. Parks can be public and/or private.	An open space available for civic purposes and commercial activities. Building frontages should define these spaces. Plazas are primarily hardscape combined with landscaping.	Trails are for pedestrian and bicycle users; they may be placed within developments and natural areas. This includes trails indicated in the PROS Plan, as well as private trails.
Frontage/ Access	Access can be restricted to protect the sensitive nature of the environment; public streets and pedestrian trails shall provide access according to existing codes.	At least two street frontages are required.	At least one street frontage is required.	Connectivity points with street sidewalks, and public access points required.
Size	Varies.	According to the PROS Plan standards.	Width: 10 feet minimum. Length: 20 feet minimum.	Width: 5 feet minimum.
Facilities to consider	Signage, trails, benches, drinking fountains, restrooms.	Signage, trails, benches, sports facilities, drinking fountains, restrooms; additional facilities according to the PROS Plan.	Seating arrangements, water features, drinking fountains, bike racks.	Signage, benches, drinking fountains.

### 3.4.5.3 Residential open space standards

1. Open spaces within residential development should provide private open space or courtyard for residents (Figure 3-25). Instead of fragmented small open spaces, open spaces should be consolidated whenever possible, in accordance with the City's open space and landscaping standards (PMC 25.175.080).

2. Residential open space shall be no less than 25 feet wide, and not less than 800 square feet in area. Developments of more than 10 units shall add 50 square feet for each additional unit.

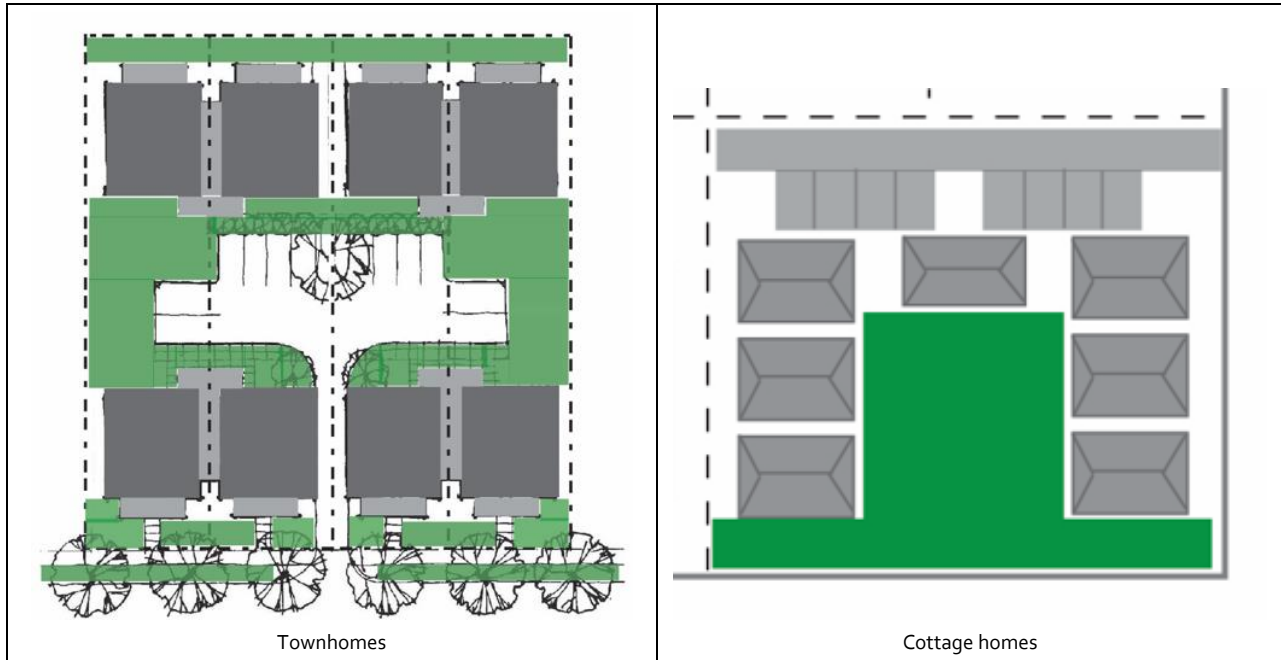


FIGURE 3-25: OPEN SPACES WITHIN RESIDENTIAL DEVELOPMENTS

### 3.4.6 RECLAMATION

#### 3.4.6.1 Intent

The intent of the Reclamation land use is to designate areas that are currently or more recently have been used for gravel mining operations for future reclamation.

#### 3.4.6.2 Standards

1. A reclamation plan and future studies shall be required to designate appropriate uses in this area.

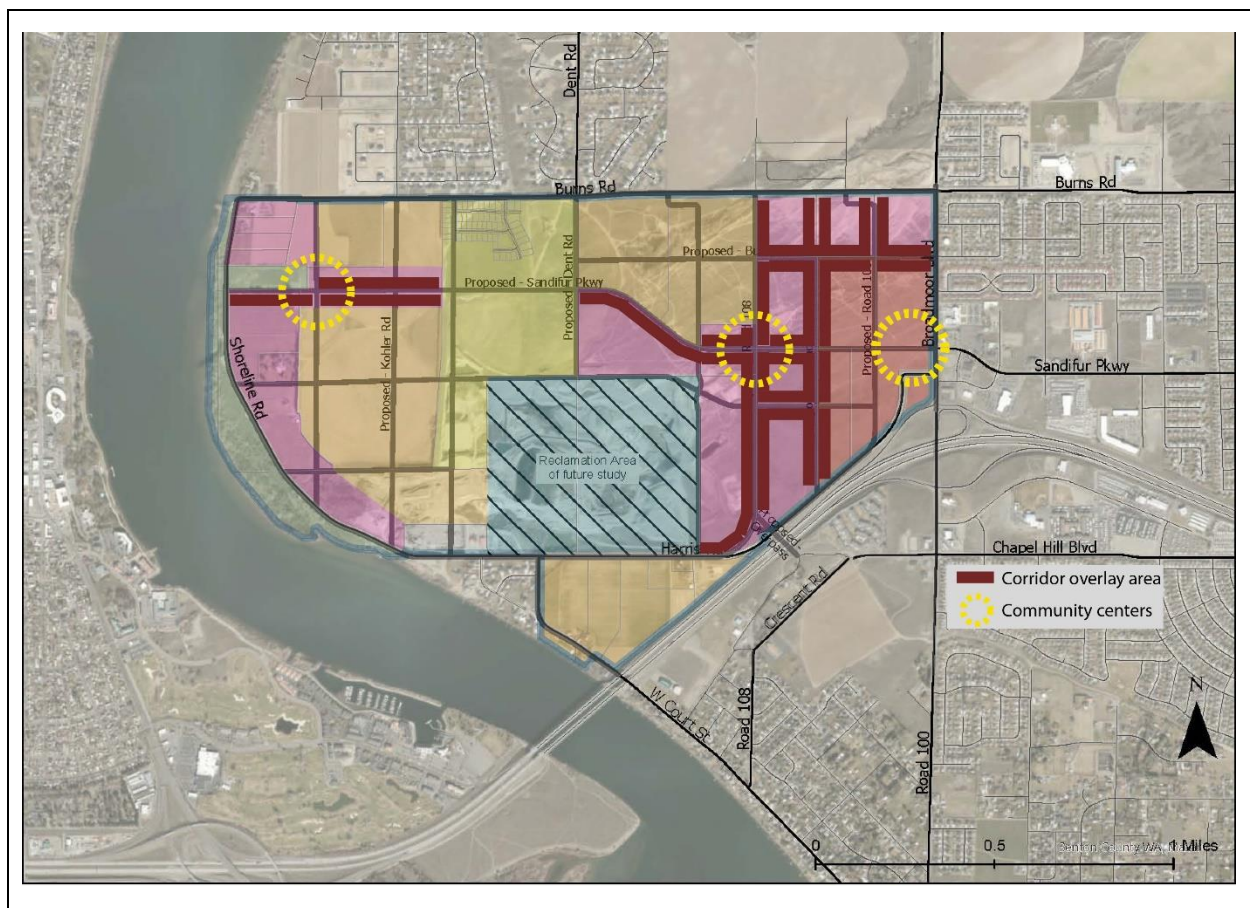


## 3.5 STANDARDS APPLICABLE TO COMMUNITY CENTERS AND CORRIDORS

### 3.5.1 INTENT

Community center and corridor standards shall apply to the center and street corridor locations in addition to all other standards in the underlying land use and zoning districts. Community center standards shall apply to abutting properties at the intersections and within 200 feet of the intersection as shown in Figure 3-26. Street corridor standards shall apply mostly to mixed-use and abutting street corridors as shown in Figure 3-26.

1. Gateway center at Broadmoor Boulevard and Sandifur Parkway intersection
2. Mixed-use center at Road 108 and Sandifur Parkway intersection
3. Waterfront neighborhood center at the future north-south roadway and Sandifur Parkway intersection



**FIGURE 3-26: BROADMOOR COMMUNITY CENTERS AND CORRIDOR OVERLAY**

### 3.5.2 GATEWAY CENTER

Gateway center shall provide a sense of entering inside a community while maintaining the retail and commercial character of the land use.

#### 3.5.2.1 Design standards for Gateway Center

Within the Broadmoor area, lots at the intersection of Broadmoor Boulevard and Sandifur Parkway shall be treated with the following:

1. Landscaping
2. Gateway signage at the intersection
3. Buildings located close to the intersection

### 3.5.3 MIXED-USE CENTER

This will provide a central location for various pedestrian-oriented uses and activities such as restaurants, shopping, transit access, housing, etc.

#### 3.5.3.1 Design standards for Mixed-use Center

1. Buildings shall be located close to the intersection.
2. Buildings shall face the corner and be accessed by pedestrians from the sidewalk (Fig 3-27).



**FIGURE 3-27: BUILDING ACCESSED FROM THE CORNER**

3. Building designs shall appropriately address building scale, mass, and form, and the use of materials, architectural elements, and details to ensure a compatible and aesthetic contribution to the built environment.

4. Parking lots shall not be allowed at the corner. Locate parking toward the interior of the site and integrate parking areas to establish an attractive built environment that provides accessibility to public spaces and buildings.
5. Sidewalks shall be minimum 10 feet wide.
6. Public spaces, plazas, and/or outdoor seating are required at each intersection.
7. Ground floor retail should have transparent façade to provide visibility.
8. Landscaping with shade trees shall be provided.
9. All other Mixed Residential and Commercial land use standards shall apply.

### 3.5.4 WATERFRONT NEIGHBORHOOD CENTER

This will provide a central location for various pedestrian-oriented uses and activities as well as access to the trails and shoreline-based recreation.

#### 3.5.4.1 Design Standards for Waterfront Neighborhood Center

1. All standards for Mixed-use Center and Mixed Residential and Commercial land use shall apply.
2. Developments should connect to the trail system to provide access to the shoreline according to the City's Linkage Plan and shoreline access as indicated in the Shoreline Master Program.
3. Developments should consider and provide visual access to the shoreline when possible.
4. Developments should maintain environmental protection.

### 3.5.5 STREET CORRIDORS

Street corridors will promote walkability and create an outdoor environment for various pedestrian-oriented uses and activities such as restaurants, shopping, transit access, housing, etc.

#### 3.5.5.1 Design Standards for Street corridors

1. All standards in Sections 3.4.3.8 and 3.4.4.2, Building Placement and Relationship with Street referring to Street corridors shall apply
2. Parking standards for corridors in Section 3.7 shall apply
3. Street Environment and Public Realm standards in Section 3.6 shall apply

## 3.6 STREET ENVIRONMENT AND PUBLIC REALM

### 3.6.1 INTENT

The intent of street standards is to create an outdoor environment reflecting the character of each land use (Figure 3-26). For example, streets within Mixed Residential and Commercial land use shall be pedestrian oriented to allow active outdoor pedestrian ambience. For mixed-use areas, street sidewalks should provide public realm with walking, shopping, seating, and various interactions. Streets should provide connectivity, accessibility, window shopping, and various recreational opportunities. For residential streets, access and circulation for both pedestrian and vehicular traffic should be considered.



**FIGURE 3-28: ACTIVE STREET ENVIRONMENT WITH OUTDOOR SEATING AND WALKING**

### 3.6.2 STANDARDS

#### 3.6.2.1 Street environment

1. Sidewalks in the Mixed Residential and Commercial and commercial land use shall be wide in accordance with Figures 3-30 through 3-35. Businesses fronting such sidewalks shall be allowed to provide outdoor seating and activities when such activities maintain a clear width according to ADA standards.
2. Streetscape shall comply with Section 3.7, Landscaping Standards
3. Mixed-use streets shall include street furnishing such as outdoor seating, benches, bike racks, artworks, planter boxes etc. Front side of mixed-use buildings shall include plazas, outdoor seating areas, and/or landscaping.
4. Building design, facades treatment and architectural details shall create visual interest at the street level. Developments standards under Sections 3.4.3.8 and 3.4.4.3, Building Placement and Relationship with Streets shall apply for an effective street environment.



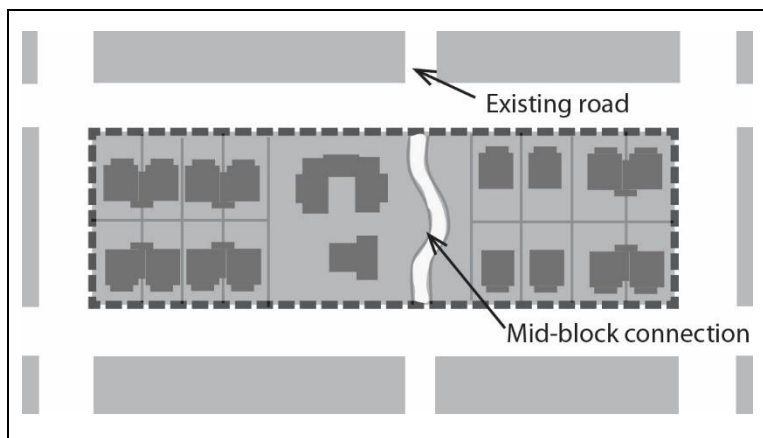
5. The City's Transportation System Master Plan (TSMP) provides lists of specific street types and design standards based on traffic demand, and the character of the land use they will serve. Based on the TSMP, Broadmoor area street and sidewalk standards shall maintain the standards illustrated in Figures 3-30 through 3-35.

### 3.6.2.2 Courtyards and plazas

1. Both private and semi-private outdoor spaces shall be incorporated in mixed-use developments. Private outdoor courtyard areas are for residents only. Semi private plaza areas for visitors shall also be provided in areas adjacent to the retail/commercial uses
2. Plazas can be located at major street intersections, and connected with sidewalks and pedestrian trails.
3. Buildings shall be oriented towards plaza areas to create an active street environment.

### 3.6.2.3 Mid-block connections

1. Mid-block connections shall be allowed through the site and through the buildings to enhance pedestrian connections through the neighborhoods (Figure 3-29).
2. They shall be used when block length exceeds the required standards.
3. Access to mid-block pathways should be clearly visible, and should be designed to invite pedestrians. They shall be connected with sidewalks.
4. Mid-block connections shall be well-lit for safety
5. They should connect with sidewalks and other street level uses such as outdoor seating and dining, art-work, water features etc.



**FIGURE 3-29: NON-RESIDENTIAL MID-BLOCK CONNECTION**

### 3.6.2.4 Alleys

1. Alleys are to be used for service access, such as garages for residential uses, and loading services for commercial uses.
2. Alleys are encouraged in all land uses.



### 3.6.2.5 Service and internal connections

1. Standards for private streets/lanes:
  - a. Private street improvements for streets providing access to uses that are not single-family residential shall meet the standards for local access roads, at a minimum, with the exception being that sidewalk must be present on at least one side and on-street parking must be present on one side. This will result in a roadway section, with curb and gutter, that measures 32 feet back-to-back of curb. Street lighting will be provided per the type chosen at the developer's discretion unless otherwise specified by the City.
  - b. Private street improvements for streets providing access to single-family residential uses shall be designed to meet International Fire Code requirements for fire apparatus, including pavement markings and signage for "No Parking – Fire Lane." Sidewalks are not required when pedestrian paths are provided with a design accommodating pedestrian circulation that is separated from vehicle traffic movements. Street lighting will be provided per the type chosen at the developer's discretion unless otherwise specified by the City.
  - c. Private streets must not interfere with vehicle, public transportation, or non-motorized access to public areas, and may not preclude the connection of the transportation system.
  - d. Stormwater facilities must be designed to treat and retain all stormwater on site without any runoff entering City right-of-way.
  - e. Every private street shall be named, and names shall be clearly posted in accordance with the Manual of Uniform Traffic Control Devices (MUTCD) standards.
2. Alleys, if used, shall maintain a minimum 20-foot width.



FIGURE 3-30: PRINCIPAL ARTERIAL STREET STANDARDS

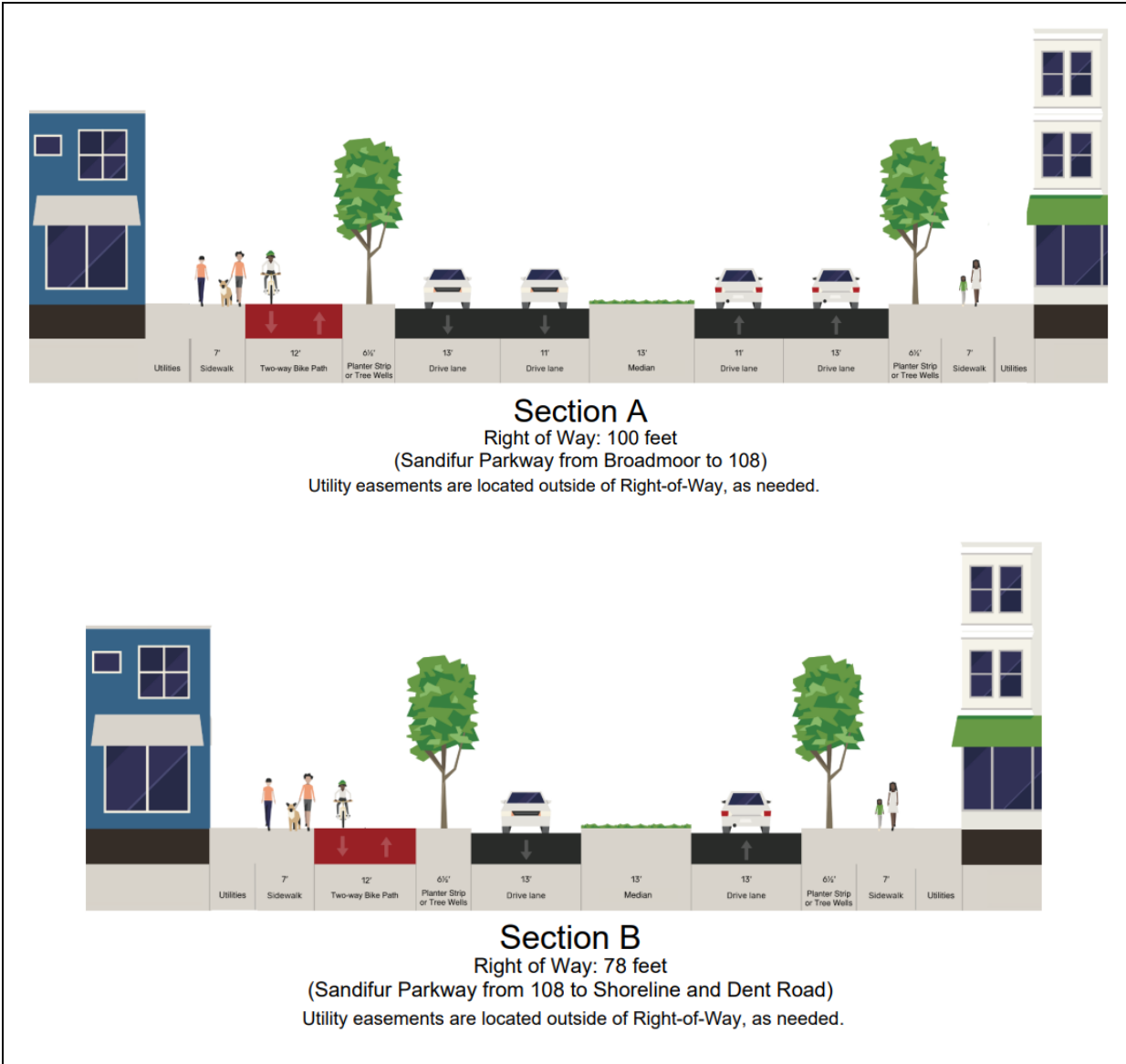
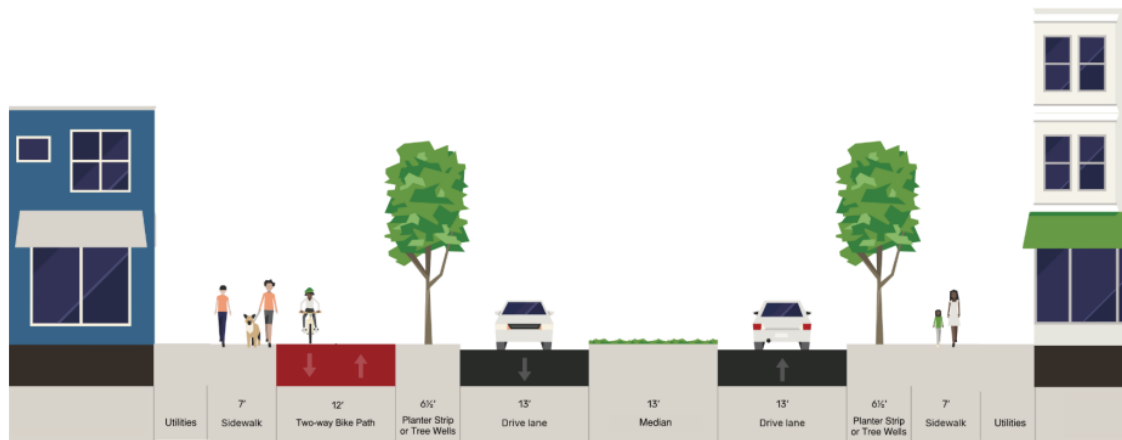


FIGURE 3-31: MINOR ARTERIAL STREET STANDARDS

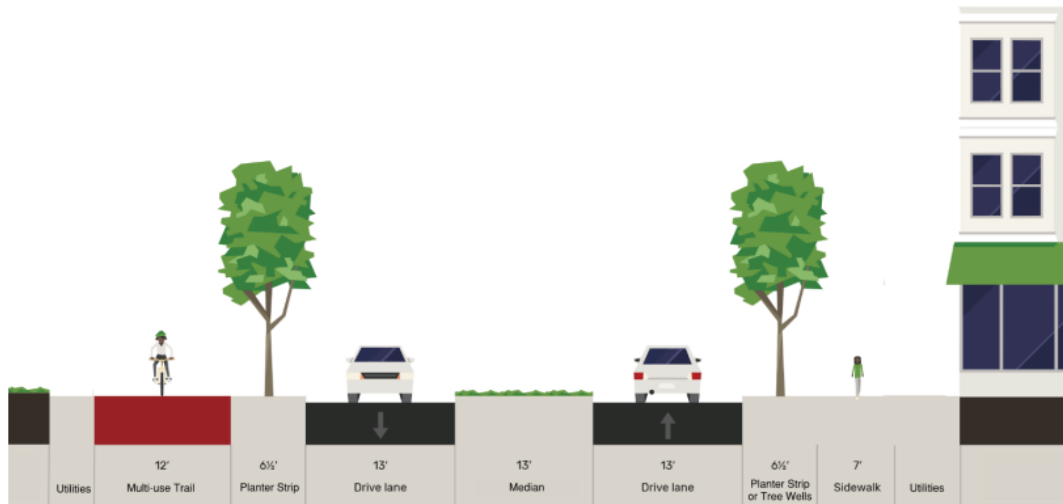


### Section A

Right of Way: 78 feet

(Road 108, Kohler Road, and unnamed East-West Collector)

Utility easements are located outside of Right-of-Way, as needed.



### Section B

Right of Way: 71 feet

(Harris Road and Shoreline Road)

Utility easements are located outside of Right-of-Way, as needed.

**FIGURE 3-32: COLLECTOR STREET STANDARDS**



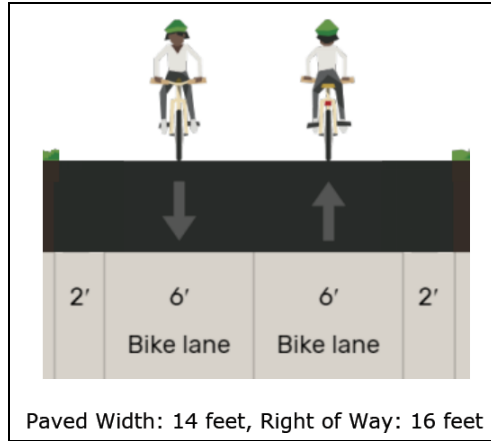
FIGURE 3-33: NEIGHBORHOOD COLLECTOR STREET STANDARDS



**FIGURE 3-34: LOCAL STREET STANDARDS**

### 3.6.2.6 Multi-use trails

Trails in the Broadmoor areas can be multi-use/ shared-use in conjunction with existing sidewalks, or standalone pathways. These will provide walking and biking opportunities. Depending on their location, they can serve both recreational and transportation needs. The path design will vary in terms of use, surface types, and widths. Hard surfaces are generally better for bicycle travel. Adequate widths should be provided to accommodate walking, biking, and maintenance vehicle access. Recommended width identified for trails is shown in Figure 3-35.



**FIGURE 3-35: MULTI-USE PATHWAY**

## 3.7 PARKING STANDARDS

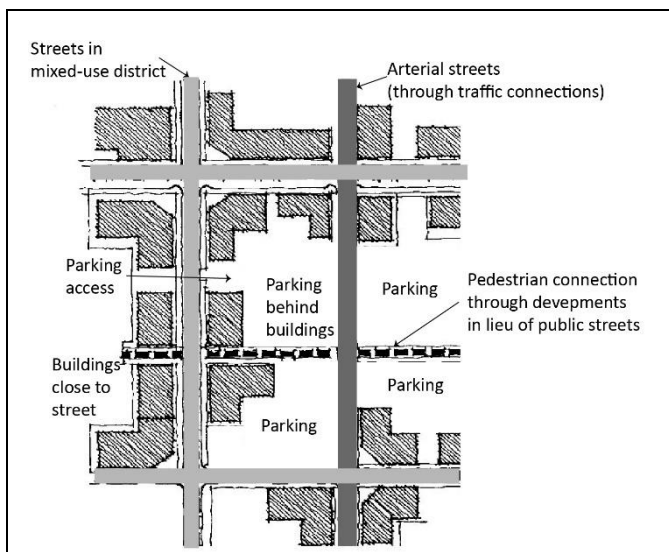
### 3.7.1 STANDARDS FOR RESIDENTIAL DEVELOPMENTS

1. Locate parking toward the interior of the site, or backside of the buildings; integrate parking areas to establish an attractive built environment that provides accessibility to public spaces and buildings
2. Two on-site parking stalls per dwelling unit is required in general except for when they are specifically mentioned in Table 3-13.
3. For multi-unit apartments containing studio units, a reduction in parking for studio units can be considered at one parking per each studio apartment.
4. An average of 25% reduction in minimum parking is allowed for all dwelling units in a multiplex within ¼ mile of transit
5. For multiplexes/ apartments, primary parking, open or covered, and garages must be located at the rear or side of the lot, or must be located in areas that are less visible or less prominent from the street. On the primary street frontage, no more than 10% of the total parking stalls, or 35% of the street frontage, whichever is less in length can be used for parking.
6. For multiplexes/ apartments, parking located near the street must be screened with a minimum of 15 ft landscaping buffer between the sidewalk and parking. Landscaping buffer shall include trees and shrubs. See Residential Site Landscaping for additional details.
7. For multiplexes/ apartments, parking must be visible and adequately lit from open areas, pedestrian walkways and dwelling units in order to ensure safety.
8. Large parking lots in apartment or multi-unit developments with more than 660 linear feet of parking shall be broken into a few smaller parking lots.

### 3.7.2 STANDARDS FOR MIXED-USE AND NON-RESIDENTIAL DEVELOPMENTS

1. Parking in the Mixed Residential and Commercial land use shall be according to the requirements in Table 3-13.
2. Residential and commercial parking shall be physically separated or clearly designated.
3. Vehicular access and driveways shall be located to minimize impact on pedestrian/bike routes and optimize regional traffic flow.
4. Parking lots shall not occupy the corner directly facing a street intersection.

5. On local, neighborhood collector and collector streets in Mixed Residential and Commercial land use, and within community centers and corridors (Figure 3-26), parking shall not be located between the street and the primary building (Figure 3-36). The City may allow an individual building or development to have surface parking on no more than 50% of the street frontage in Commercial land use, and no more than 30% on the centers and corridor streets (Figure 3-26) and according to Section 3.2.3.2, Street Connectivity Standards. Parking garages can be included in the count as buildings when they do not consist of more than 25% of the site or street frontage. These standards shall also be applicable for arterial and collector street frontages when they are the main street frontages.
  - a. In cases when parking abuts the street, at least 15 feet buffer between the street and the parking lot shall be used. Such buffer shall include sidewalks, plazas, landscaping etc. (Figure 3-37). Corners of the parking lots facing a public intersection shall also be treated with a minimum of 75' square-feet of public amenities, green space, landscaping, benches, etc.



**FIGURE 3-36: PARKING BEHIND THE BUILDING IN MIXED-RESIDENTIAL AND COMMERCIAL LAND USE**

- b. Single buildings containing single use or business that exceed 100,000 square feet (first floor only), and/or when the site faces three or more public street frontages may apply for an exemption to standards in 3.7.2(5) in Commercial land use. In such cases following standards shall apply:
    - i. Street frontage may be counted as an aggregate of all public street frontages instead of each public street for surface parking, and 50% of the aggregated public street frontages can front surface parking
    - ii. The site shall include additional landscaping and public space treatment along the frontages, with a minimum of a 20' buffer and 200 square feet of

public/pedestrian amenities at intersection corners. Additionally, the landscaping buffer shall include street trees at every 100 ft.

- iii. Parking lot shall include clearly designated and enhanced walkways at every 120 ft; such walkways shall be treated with separate pavement material, landscaping and/or pedestrian striping.
  - iv. Safe pedestrian walkways shall be provided from the parking lot to the building and public sidewalks.
  - v. All standards of 3.8.3, Parking lot landscaping shall apply.
6. Portions of site frontages that are devoted to off-street parking or access shall include design features such as landscaping screening to maintain visual continuity of the street and sidewalk to the fullest extent possible (Figure 3-37).
  7. Parking lots shall include bicycle racks near the sidewalk or close to the building.



**FIGURE 3-37: PARKING LOT SCREENED WITH LANDSCAPING AND ARTWORK**

8. On-street parking or off-street public parking lots may be used in combination with dedicated off-street parking to accommodate parking demand from individual developments.
9. Parking lots among abutting parcels/businesses shall be connected.
10. Developments shall comply with Section 3.8.3, parking lot landscaping and buffer standards.
11. Parking lots shall have pedestrian connectivity e.g. walkways, mid-block connections etc. at every 250 ft.



12. Coordinate with public transportation provider for conformance with planned transit systems to the transportation network and plan for the integration of appropriate transit facilities and infrastructure.
13. Parking lots near transit centers shall include shared parking between businesses and transit riders. In such cases, staggering time of the share parking lots shall be considered.

### 3.7.3 PARKING REDUCTIONS

1. The number of parking stalls required can be lowered by 15% for parking areas greater than 16,000 square feet.
2. If adjoining developments can be developed and designed efficiently and economically as one facility, the total combined required parking stalls may be reduced by 10%.
3. The required number of parking stalls can also be reduced by 20% if the developer includes access to bus stop on a transit route, or shared parking spaces with adjacent developments.
4. For structured parking, up to 20% reduction in required parking shall be allowed.
5. Parking lots larger than the minimum required in the code should have a 10% increase in the total landscaping area, number of trees, shading, and pedestrian walkways in addition to other design standard requirements.

**TABLE 3-13: PARKING STANDARDS IN MIXED RESIDENTIAL AND COMMERCIAL LAND USE**

Use Category	Minimum	Maximum
<b>RESIDENTIAL USES</b>		
Single-family detached	1	2
Accessory dwelling units	0.5	1
Single-family attached		
Townhomes/ row houses	1	2
Duplex, triplex, fourplex	1 per du	2 per du
Apartments/ Multifamily dwellings/ Condominiums	0.75 per du	1.5 per du
Manufactured homes/ factory-assembled homes	According to the type of developments, single-family or multiplex	
Nursing homes and assisted living facilities	0.25 per bed	0.5 per bed
Adult family home (6 or fewer)	Same as single-family requirements	
Family home day care facility	Same as single-family requirements	

Use Category	Minimum	Maximum
<b>COMMERCIAL, OFFICE, EDUCATIONAL, AND GOVERNMENT USES</b> (per 1,000 square feet of gross floor area unless otherwise specified)		
Churches, places of worship, clubs, fraternal societies	1 per 100 square feet main assembly area	1 per 60 square feet of main assembly area
Commercial lodging (hotel, motel, bed and breakfast, short-term vacation rentals)	0.5 per room	1 per room
<b>EDUCATIONAL USES</b>		
Elementary schools	1 per classroom and 1 per employee	1.5 per classroom
Middle schools	1 per classroom	2 per classroom
High school	7 per classroom	10.5 per classroom
Universities, colleges, business, professional, technical and trade schools	0.3 per full-time student and 0.8 per employee	0.5 per FTE student and 0.8 per employee
Gyms or fitness centers	3	5
Museums and art galleries	2.5	4
Offices: administrative, professional, government	2	4
Medical and dental offices	Same as retail and services	
Libraries	Same as offices	
Community event centers	Based on site planning standards	
Portable food vendors/food trucks	None required	None required
Restaurants/bars/ breweries, wineries, and distilleries	0.5 per 3 seats	1.0 per 3 seats
Retail sales and services	3	5
Car dealership (showroom sale only)	Same as retail and services	
Car wash, repair	Same as retail and services	
Filling stations ((gasoline and/or electric)	Same as retail and services	
Veterinarian clinics for household pets (no boarding or outdoor treatment facilities)	Same as retail and services	
<b>RECREATIONAL AND ENTERTAINMENT USES</b> (per 1,000 square feet of gross floor area unless otherwise specified)		
Public and private parks and trails	To be determined during land use approval process	
Event entertainment (indoor or outdoor)	1 per 8 seats	1 per 5 seats
Theaters	1 per 4 seats	1 per 2.7 seats

Use Category	Minimum	Maximum
<b>Institutional Uses</b>		
Hospitals	1 per bed	1.5 per bed
Police and fire stations	2	4

## 3.8 LANDSCAPING STANDARDS

### 3.8.1 GENERAL STANDARDS

1. Developments shall conform with the landscaping standards of PMC 25.180, especially PMC 25.180.060 for streets in the Broadmoor area, and PMC 25.180.070, Parking Lot Landscaping.
2. Landscaping should maintain the selection of plants listed from the Washington Native Plant List for Franklin County (wnps.org). Xeriscape methods for drought-tolerant landscaping should be used. Plant material shall be a mixture of drought-tolerant deciduous and evergreen trees. Drought-tolerant plant materials may contribute up to 50% of the required landscaping ground cover and street trees.
3. Preservation of mature trees should be considered during any development. .
4. Replacement of existing trees shall be 1:1.5 ratio.

### 3.8.2 STREET FRONTAGE

1. Street trees shall be planted along public streets as shown in Section 3.6, Street Environment and Public Realm.
2. Developments must include street trees aligned in clusters of varied spacing or equal spacing depending on the street and neighborhood character.
3. Where street trees are planted with equal spacing, spacing shall be 30 feet maximum from each center.
4. Street tree minimum size at installation should be 1.5-inch caliper.
5. Street tree locations must be coordinated with the street lighting and utility plans to minimize interference.
6. All landscaping elements, plant materials, and street trees shall be planted or installed by the developer and maintained by the property owners or business owners. A maintenance agreement among adjoining property owners shall be required; the agreement shall run with the land.

### 3.8.3 PARKING LOT LANDSCAPING AND BUFFERS

1. The parking lots must be landscaped with large canopy type trees as listed in the recommended tree listing or as approved by the City, that provide shade without obstructing visual access to businesses. Trees should be allowed to mature and be

maintained a minimum height of 30 feet. At least 40% of the entire parking lot must be shaded by these trees within 12 years of planting.

2. Large parking lots shall be broken into smaller ones with landscaping and pedestrian connections. A landscaped area shall be within 75 feet of any parking stall.
3. Safe pedestrian walkways shall be provided from the parking lot to the building and public sidewalks.
4. Parking lots between the building and an arterial street shall have a minimum buffer of 15 feet between the parking lot and the property line. Single buildings in Commercial land use exceeding 100,000 square feet (first floor only) that apply for an exemption according to 3.7.2(5) shall include additional landscaping and buffer according to 3.7.2(5)(b).
5. Parking lots adjacent to local access streets and alleys shall have a minimum buffer of 5 feet between the parking lot and the property line.
6. All developments shall provide interior parking landscape areas at the following rates:
  - a. If parking lots have 10 to 30 parking stalls, then 20 square feet of interior parking landscape per parking stall shall be provided.
  - b. If parking lots have 31 to 50 parking stalls, then 25 square feet of interior parking landscape per parking stall shall be provided.
  - c. If parking lots have 51 or more parking stalls, then 30 square feet of interior parking landscape per parking stall shall be provided.
7. Interior parking lot landscape areas distributed as planting islands or strips shall have an area of at least 100 square feet and a narrow dimension of no less than 5 feet in any direction.
8. Priority for location of interior parking lot landscaping shall be:
  - a. First, at the interior ends of each parking bay in a multiple lane parking area. This area shall be at least 5 feet wide and shall extend the length of the adjacent parking stall. This area shall contain at least one tree.
  - b. Second, with the remaining required square footage of landscape area being equally distributed throughout the interior of the parking lot either as islands between stalls or landscape strips between the two rows of a parking bay or along the perimeter of the parking lot. Interior landscape islands shall be at least 5 feet wide and shall extend the length of the adjacent parking stalls. Landscape strips must be a minimum 5 feet wide.



## 3.9 ENVIRONMENTAL PROTECTION

### 3.9.1 STANDARDS

Any developments in the environmentally sensitive areas shall be avoided unless they are mitigated according to local, state, and federal regulations.

1. Developments shall comply with the City's Critical Areas Ordinance and Shoreline Master Program regulations as applicable. Mitigation of existing PHS areas has been coordinated with WDFW. This mitigation plan in the Broadmoor Area EIS shall be followed for environmental protection.
2. Install and use stormwater best management practices (BMPs) to include control structures (i.e., silt fencing, etc.) (refer to Stormwater Management Manual for Eastern Washington, August 2019; <https://apps.ecology.wa.gov/publications/documents/1810044.pdf>) to prevent the likelihood of construction and stormwater runoff degrading surface waters and adjacent wetlands/riparian habitats. Control structures shall be properly maintained and inspected on a regular basis to ensure the integrity of BMPs.
3. Monitor the adverse impact and the compensation projects and take appropriate corrective measures on a periodic basis as required.

## 4 COST ESTIMATE

**You will find in this Section:**

- Cost estimate for key improvements

## 4.1 KEY IMPROVEMENTS

The City has identified priority areas for development and costs of capital facilities projects. Table 4-1 identifies cost estimates for some major capital projects. This is provided only with available data, and the actual cost of the entire area is expected to be higher, different, and/or subject to change. The City is also assessing ways to finance projects with tax revenue, bonds, and TIF.

**TABLE 4-1: COST ESTIMATE**

Capital Facilities	Estimated Cost (\$)
<b>Water</b>	
West Pasco Water Treatment Plant expansion – Phase 1 and 2 <sup>1</sup>	2,700,000
West Pasco Water Treatment Plant expansion upgrade <sup>1</sup>	910,000
New Storage Reservoir: 3.5 MG - Zone 3 <sup>1</sup>	7,469,000
Water transmission line (Transmission Main from West Pasco Water Treatment Plant to Broadmoor area) - 24 in <sup>2</sup>	3,221,000
Water transmission line (Transmission Main from West Pasco Water Treatment Plant to Broadmoor area) - 18 in <sup>2</sup>	1,985,000
Backbone Transmission Main (12,16,24 inch) <sup>2</sup>	6,961,000
	3,174,000
	4,832,000
<b>Subtotal</b>	<b>31,252,000</b>
<b>Sewer</b>	
West Broadmoor Sewer Main <sup>3</sup>	6,116,000
West Broadmoor Lift Station <sup>3</sup>	2,935,000
Sewer trunk line from Sandifur Parkway Extension to Desiree Street	4,598,000
Regional/Broadmoor Area Lift Station (includes the force main) <sup>2</sup>	3,500,000
Gravity Sewer Main – Extension of Harris Rd Sewer <sup>2</sup>	9,169,000
Gravity Sewer Main – Regional Lift Station Basin <sup>2</sup>	
8 inch	5,315,000
12 inch	7,138,000
15 inch	6,167,000
<b>Subtotal</b>	<b>44,938,000</b>
<b>Street, Park, and Fire Improvements</b>	
Harris Road/Road 108 <sup>4</sup>	9,909,450
Sandifur Parkway Extension from Broadmoor Boulevard to future Road 108 <sup>4</sup>	7,363,350
Sandifur Parkway Extension from future Road 108 to future Dent Road <sup>4</sup>	5,146,450
Broadmoor Boulevard improvements <sup>4</sup>	2,676,300

<b>Capital Facilities</b>	<b>Estimated Cost (\$)</b>
Broadmoor Intersections <sup>4</sup>	1,927,000
Future Buckingham Drive <sup>4</sup>	4,804,900
Burns Road from Broadmoor Boulevard to future Road 108 <sup>4</sup>	2,718,950
Burns Road from future Road 108 to future Dent Road <sup>4</sup>	2,725,750
Harris Road extension and east west road between Harris Road and Road 108 <sup>4</sup>	7,274,850
<b>Subtotal</b>	<b>44,547,000</b>
I-82 Loop Ramp <sup>4</sup>	6,000,000
Parks <sup>5</sup>	6,100,000
Fire Station <sup>5</sup>	500,000
<b>Subtotal</b>	<b>9,900,000</b>
<b>Total</b>	<b>143,237,000</b>

Sources:

1. Pasco Comprehensive Water System Plan 2019
2. Technical memorandum from Murraysmith on November 18, 2019; updated January 6, 2020 – 6 year planning horizon; this includes an expanded study area outside of the Broadmoor boundary
3. City of Pasco 2021 Comprehensive Sewer Plan Addendum
4. Tax Increment Financing estimates from the City
5. Pasco CIP 2020- 2025

## 5 IMPLEMENTATION

**You will find in this Section:**

- Partnership
- Financing
- Phased development
- Development regulations
- Permit review process



The Broadmoor Master Plan will be implemented through various tools and policies by the City and its partners.

## 5.1 PARTNERSHIP

The City and property owners should partner in developing the area with common goals and vision. A public-private partnership will require cost sharing between the City and developers, locating catalytic projects in the area, and identifying and allocating capital funds for the development of this area. Other partnership activities will include marketing the area for a business-friendly environment, welcoming businesses, and promoting innovative businesses.

The City will partner with other public and private agencies.

- Benton Franklin Transit Authorities - transit centers, transit stops, and a pedestrian-friendly environment
- Pasco School District – to provide school sites, share school and park facilities
- Port of Pasco – jobs and tourism in this area
- Franklin County – regional collaboration
- Visit Tri-Cities – jobs and tourism
- Pasco Chamber of Commerce – business development
- Pasco Housing Authority, Homebuilders’ Association – promote various housing choices

## 5.2 FINANCING

Financing capital projects will be the key in implementing the master plan vision. The City has already allocated funds in its Capital Improvement Program (CIP) for major roads, water, and sewer infrastructure. Additional financing is being planned through TIF.

### 5.2.1 TAX INCREMENT FINANCING

According to RCW 39.114, TIF provides a financing tool that local governments in Washington State (defined as cities, towns, counties, port districts, or any combination thereof) can use to fund public infrastructure in targeted areas to encourage private development and investment. The City of Pasco may identify Broadmoor as the “increment area.” Once identified, the property tax portion of increases in assessed value of properties within Broadmoor area can be allocated toward paying for the public improvement and infrastructure costs of this area.

The City is assessing the use of this tool to finance various transportation projects included under street improvements in Table 4-1 in the previous section.

## 5.3 PHASED DEVELOPMENT

The Broadmoor area will be developed in multiple phases based on the local market conditions, investments, and financing tools as they become available for this area. Initial phase developments will occur on the east side near Broadmoor Boulevard. Later phases will take place farther to the west. However, developments on the west side near the Columbia River would likely occur concurrently with the developments near Broadmoor Boulevard.

## 5.4 DEVELOPMENT REGULATIONS

Development regulations under Section 3 of this document will be implemented in coordination with other municipal codes. When there is a conflict, the more restrictive regulation, or the regulation that better implement the vision of the Broadmoor area, will apply.

## 5.5 PERMIT REVIEW PROCESS

The City will identify and designate zoning consistent with all the land uses designated in this master plan. Zoning and land use shall comply with this master plan and development regulations, and other applicable PMC regulations.

Development permit process shall undergo a regular site plan permit process for all non-residential developments in Commercial and Open Space land uses, and residential developments of multi-unit developments in Medium Density Residential land uses according to Section 5.5.1. Residential development such as subdivision applications within Medium Density Residential land use shall comply with Pasco's subdivision regulations (PMC Title 21) and related application processes. Developments in the Mixed Residential and Commercial land use shall follow requirements in Section 5.5.2..

### 5.5.1 SITE PLAN REVIEW PROCESS

#### 5.5.1.1 Submittal Requirements

The following information must be submitted as deemed applicable by the Community and Economic Development Director based on the size, scale, and complexity of the master plan:

1. Site description.
  - a. Physical characteristics of the site such as slope, vegetation etc.
  - b. Infrastructure facilities and capacity, including water, sewer, roads
2. Existing Site Conditions Map. The existing site conditions map must include the following information on site and within 150 feet of the proposed master plan (as applicable):

- a. The applicant's entire property and the surrounding property. The property boundaries, dimensions and gross area must be identified. Existing aerial photos may be used;
  - b. Ownership of master plan area and ownership of all property within 150 feet of the proposed master plan;
  - c. Topographic contour lines shown at one-foot intervals for slopes of 10 percent or less. For slopes greater than 10 percent, contour lines must be shown at two-foot intervals. Slopes greater than 25 percent must be identified;
  - d. The location, names, and widths of existing public and private streets, alleys, drives, sidewalks, bike lanes, multi-use trails, transit routes and facilities, rights-of-way, and easements. Existing aerial photos may be used;
  - e. The location of existing irrigation canals and ditches, pipelines, drainage ways, waterways, railroads, and any natural features such as rock outcroppings, wooded areas, and natural hazards. Existing aerial photos may be used;
  - f. The location of existing sanitary and storm sewer lines, water mains, septic facilities, culverts, and other underground and overhead utilities;
  - g. Potential natural hazard areas, including any areas identified as subject to a 100-year flood, areas subject to high water table, and areas mapped by the City, County, or State as having a potential for geologic hazards;
  - h. Resource areas, including wetlands on the City's Local Wetlands Inventory, streams, surface mines, and wildlife habitat identified by the City or any natural resource regulatory agencies as requiring protection; and
  - i. Locally or federally designated historic and cultural resources on the site and the adjacent parcels
  - j. Date, north arrow, scale, names, and addresses of all persons listed as owners on the most recently recorded deed.
  - k. Name, address, email address, and telephone number of project designer, engineer, surveyor, and/or planner, if applicable.
3. Proposed Site Plan
- a. Description of the following:
    - i. Development boundary subject to Broadmoor area master plan

- ii. Project description, types of used proposed, approximate location, and approximate timing of each proposed phase of development. The phasing plan may be tied to necessary infrastructure improvements. May also reference submitted maps or diagrams
  - iii. How the proposed water, sewer, and street system will serve the size and type of development and uses planned for this area
  - iv. How the location and sizing of water and sewer facilities on site will be consistent with existing and planned facilities
  - v. How water flow volumes will be provided to meet fire flow and domestic demands
  - vi. The function and location of any private utility system
  - vii. Description of adjacent land uses within ¼ mile distance
- b. Scaled maps or diagrams that include the following information (as applicable):
  - i. Development boundary
  - ii. Phasing plan with land uses
- c. Site plan drawing shall include the following:
  - i. Property boundaries and dimensions
  - ii. Adjacent property(s) land use and zoning
  - iii. Names and location of all existing streets abutting the site
  - iv. Building envelopes and square footage
  - v. Distance from property lines and existing structures to building envelope area
  - vi. Vehicle, biking, and walking circulation system, including cross-sections, and where these facilities will connect with the existing and planned system
  - vii. Transit routes and facilities within ¼ mile
  - viii. Parking, loading, and service areas including loading and service areas for waste disposal
  - ix. Parking lot area(s) with proposed parking stall count including the number of proposed accessible spaces. Must include proposed landscape island locations

- x. Location and size of existing and/or proposed public sidewalks, curbs and gutters adjacent to or on the property
  - xi. Pedestrian connections
  - xii. North arrow and scale
  - xiii. Location, type and size of any easements
  - xiv. Location and dimensions of proposed and/or existing driveway approaches, mid-block connections, and/or any internal circulation
  - xv. Other information necessary to show how the site plan meets the Broadmoor master Plan
- d. Landscape plan including parking lot landscape, groundcover (soft or hard surface), tree and plant species, size at maturity
  - e. Architectural concept plan (e.g., information sufficient to describe architectural styles, building heights, and general materials); and
  - f. Sign and lighting concept plan (e.g., locations, general size, style, and materials of signs).
  - g. SEPA checklist as applicable

#### 5.5.1.2 Administrative Review

1. Site plan application shall be reviewed by appropriate City staff once a complete application is received
2. All public notifications and agency coordination shall follow the State and City rules including SEPA notification rules
3. The City staff shall review the application for compliance with the Broadmoor Master Plan and development regulations, and how it meets the vision of the master plan.

### 5.5.2 MIXED USE PROJECT REVIEW PROCESS

#### 5.5.2.1 Single Mixed-Use Project (Vertical and Horizontal):

A mixed-use project that is either vertical or horizontal and contained within a single building and single site shall be reviewed through the site plan review process indicated in Section 5.5.1.

#### 5.5.2.2 Multiple Building / Multiple Site Mixed-Use Development Projects:

If the project is horizontally-integrated on more than one site and/or in more than one building, a mixed-use master site plan is required according to Section 5.5.2.3 in addition to all the site plan

requirements in Section 5.5.1. This is to ensure that the buildings are functionally and physically integrated.

#### 5.5.2.3 Mixed-Use Master Site Plan Requirements.

1. Initial development of a multiple building mixed-use site or phased development of a mixed-use site shall require submittal and approval of a mixed-use master site plan, unless it is already subject to a previous mixed-use master site plan approval. Mixed-use master plans shall include the following:
  - a. All submittals according to the site plan review requirements.
  - b. Mapping and written description of the mix of uses, such as residential and commercial or non-residential portions.
  - c. If applicable, description of proposed future phasing.
  - d. Written description of the proposal's consistency with transportation and other service requirements.
  - e. A parking plan with written descriptions and graphics describing how residential and non-residential uses can provide sufficient and coordinated parking to avoid impacts on adjacent areas.
2. The overall mix of land uses within a Mixed-Commercial/ Residential land use area (Figure 3-14) shall be reviewed to allow a minimum of 20% residential and 20% of non-residential use according to Section 3.4.1.1 (4). Proximity to another use can be accounted for in the mix of uses according to Sections 3.4.1.1 (5) and 3.4.3.2 (1).
3. The Planning Director may allow flexibility in the site use or allocation of use if the project meets the overall intent of the Broadmoor area.

## 5.6 MONITORING OF THE PLAN

A periodic monitoring system shall be established by the City to ensure the developments implement the vision of the Broadmoor area. The City shall develop an inventory of projects completed, permits approved and permits applied for in the Broadmoor area. Based on the status of developments, the City shall evaluate if the projected goals for the Broadmoor area are being met. The City may assess any implementation challenges and develop strategies such as plan amendment, budget modifications, updated review processes etc.



## 6 DEFINITIONS

**Articulation:** Shifts in the plane of walls, setbacks, step backs, overhangs, and details in order to create variation in a building façade and divide large buildings into smaller identifiable sections.

**Main street frontages:** Building facing the street of primary access, also the addressed side.

**Mid-block pedestrian connections:** Mid-block connections are pedestrian and bike link on an easement or public land connecting two sides of a block. They are usually located in the middle of a block and increase access to neighborhoods.

**Mixed use:** Mixed use development can be defined as developments integrating two or more land uses, such as residential, commercial, and office that support each other with a strong pedestrian orientation. Mix of uses can be on a site in multiple buildings (horizontal), or in individual buildings (vertical), or a combination of both.

**Mixed use, horizontal:** Development that incorporates residential and pedestrian-oriented commercial or non-residential uses (and any additional uses allowed in the underlying zoning district) on the same lot or contiguous lots in the same zone. This type of development may occur within a single building where uses are separated by walls or within multiple buildings.

**Mixed use, vertical:** Development that incorporates residential and pedestrian-oriented commercial or non-residential uses (and any uses allowed in the underlying zoning district) within the same zone. This type of development may occur within a single building or multiple buildings on the same site or contiguous sites where uses are separated by floors.

**Modulation:** Variation in the building mass through the use of step backs, setbacks, diminishing upper floor areas, and/or projecting roof overhangs.

**Private Street:** Internal circulation roads and driveways that serve individual businesses and connect with businesses internally within a site or development.

**Upper level step backs:** Buildings from the ground floor to upper level floors are pushed back toward the center of the property in order to create variation in building mass.

## 7 REFERENCES

Ecology (2019). Stormwater Management Manual for Eastern Washington, August 2019. Washington State Department of Ecology, Water Quality Division, Publication 18-10-044. Available online at: <https://apps.ecology.wa.gov/publications/documents/1810044.pdf>. Accessed: December 23, 2021

Municipal Services and Resource Center ([mrsc.org/Home/Explore-Topics/Planning/Development-Types-and-Land-Uses/Mixed-Use.aspx](http://mrsc.org/Home/Explore-Topics/Planning/Development-Types-and-Land-Uses/Mixed-Use.aspx))

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Pasco Parks, Recreation and Open Space Master Plan Draft, 2022

Pasco Transportation System Master Plan 2022

WDFW. (2018a). Priority Habitats and Species (PHS) on the web. Available online at: <https://wdfw.wa.gov/conservation/endangered/status/SE/>. Accessed June, 2018

## **APPENDIX A**

### **ENVIRONMENTAL IMPACT STATEMENT**